

Design Guidelines for Village Scale TOD



Design Guidelines for a Village Scale TOD



Fig. 2.1 A typical suburban village center 'Main Street'



Fig. 2.2 Photo simulation of the type of development TOD zoning would enable and encourage



Fig. 2.3 A typical suburban village center 'Main Street'



Fig. 2.4 Photo simulation of the type of development TOD zoning would enable and encourage

Challenges for many suburban village centers: competition between pedestrians and traffic; deterioration of public realm of sidewalks; unattractive and inappropriately scaled development.

Objectives of village scale TOD design guidelines: create an attractive and pedestrian-friendly place where automobile parking and traffic is well managed; promote attractive streets; encourage context appropriate developments that are oriented towards the street.

Design Guidelines for a Village Scale TOD

1. Development Standards.

The following standards are divided into three categories (less than 10,000 sq. ft., between 10,000 and 20,000 sq. ft. and greater than 20,000 sq ft) with the associated regulations:

Small Infill Site (< 10,000 sq. ft) (see Summary Diagrams, Fig 4.1,4.2,4.3)

- a. Minimum lot size: 5,000 sq. ft.
- b. Maximum lot size: 10,000 sq. ft.
- c. Minimum frontage: 50 feet
- d. Minimum setbacks:
 - Front yards: 0 feet
 - Rear yard: 20 feet
 - Side yard: 0 feet
 - From residential district: 20- 30 feet
- e. Maximum Building Height: 3 stories and 32 feet to setback and 4 stories and 45 feet to uppermost roof or parapet, as measured from the average established grade of the curb of adjoining streets, provided that no less than 50% of the perimeter of the property directly abuts public streets. Where less than 50% directly abuts public streets, building height shall be measured from the average level of the finished ground surface adjacent to the exterior walls of the building. (Fig. 5.1)
- f. Minimum Building Height: 14 feet to top of front wall
- g. Maximum Building Coverage: 50%.
- h. Maximum Floor Area: total floor area (exclusive of below-grade parking) shall not exceed 1.5 floor area ratio. Residential use: no limit except 500 sq ft minimum per dwelling unit within the massing envelope.
- i. Any permanent public open space that satisfies the open space requirement is not subject to the building setback (front and rear yard) limits.

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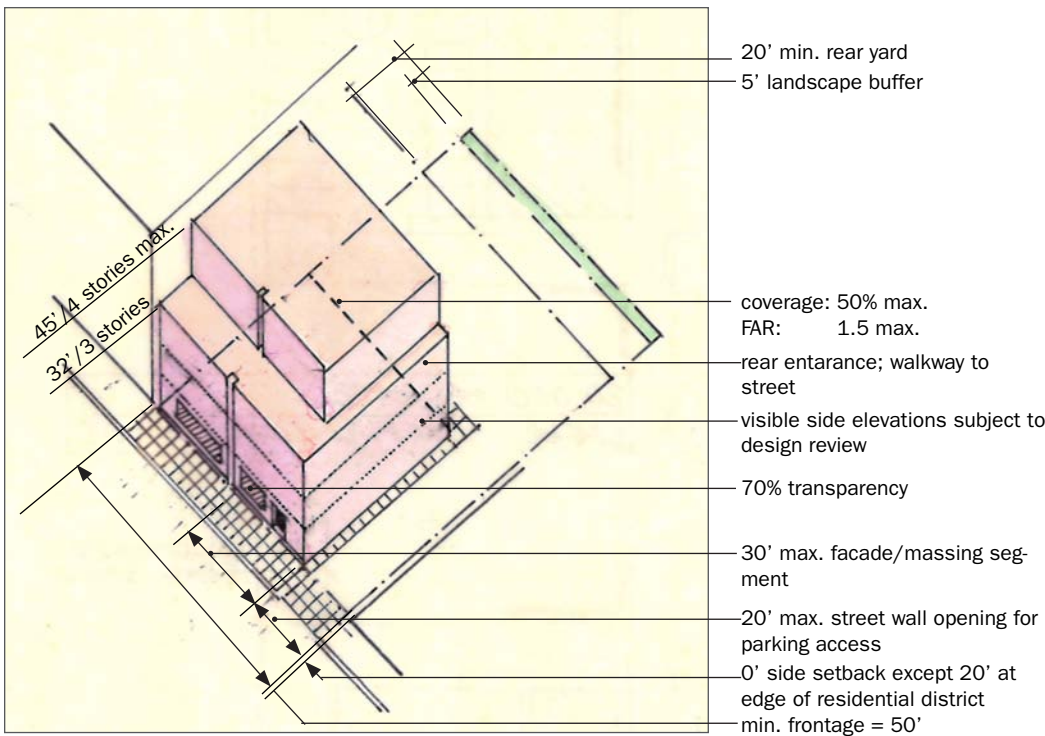


Fig. 4.1: Summary diagram for typical small infill site (10,000 sf lot or less)

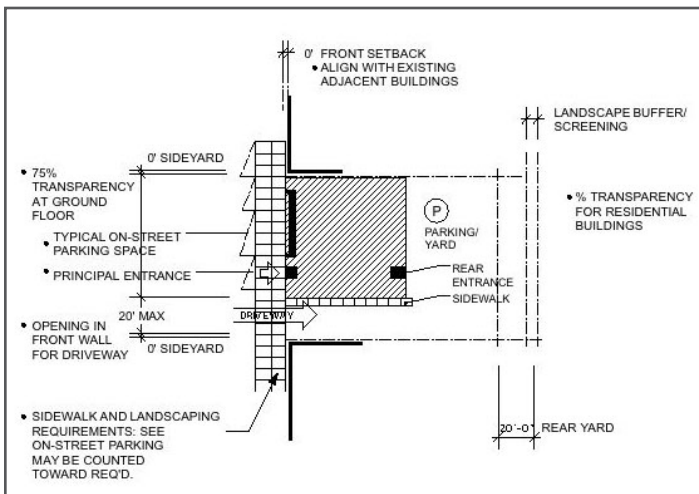


Fig. 4.2: Site plan for typical small infill site (10,000sf site or less)

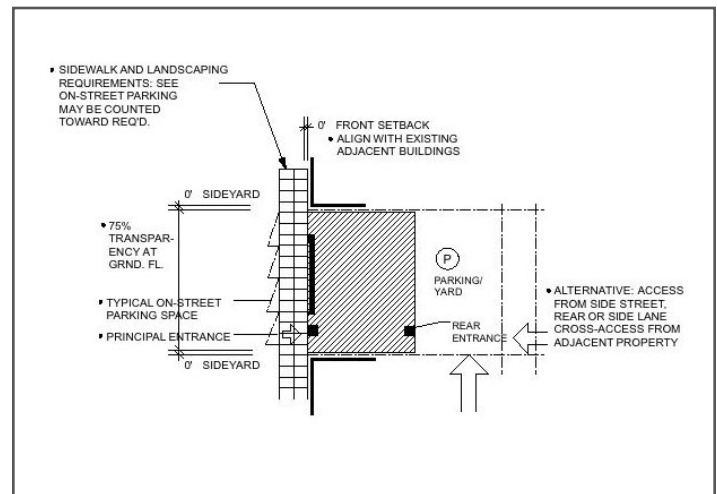


Fig. 4.3: Site plan for typical small infill site showing alternative access to parking.

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Sites between 10,000 and 20,000 sq. ft: (see Summary Diagrams, Fig 6.1, 6.2, 6.3, 6.4)

- a. Minimum lot size: 10,000 sq. ft.
- b. Maximum lot size: 20,000 sq. ft.
- c. Minimum frontage: 50 feet
- d. Minimum yards:
 - Front: 0 feet
 - Rear: 20 feet
 - Side: 0 feet, but at least 4 feet if provided
- e. Maximum Building Height: 3 stories and 32 feet to setback and 4 stories and 45 feet to uppermost roof or parapet, as measured from the average established grade of the curb of adjoining streets, provided that no less than 50% of the perimeter of the property directly abuts public streets. Where less than 50% directly abuts public streets, building height shall be measured from the average level of the finished ground surface adjacent to the exterior walls of the building. (Fig. 5.1)
- f. Maximum Building Coverage: 50%, provided that canopies and other roof coverings over walkways and streets shall not be included, and also parking structures, suitably screened and landscaped, with roof or upper-deck elevation not exceeding five (5) feet above average street grade shall not be included in the calculation of building coverage.
- g. Maximum Floor Area: total floor area (exclusive of below-grade parking) shall not exceed the amount of 1.5 floor area.

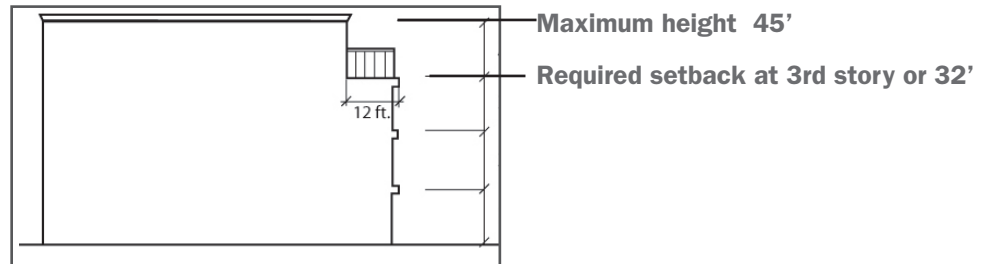


Fig. 5.1 : Maximum height and required step-back at 3rd floor and 32 feet.

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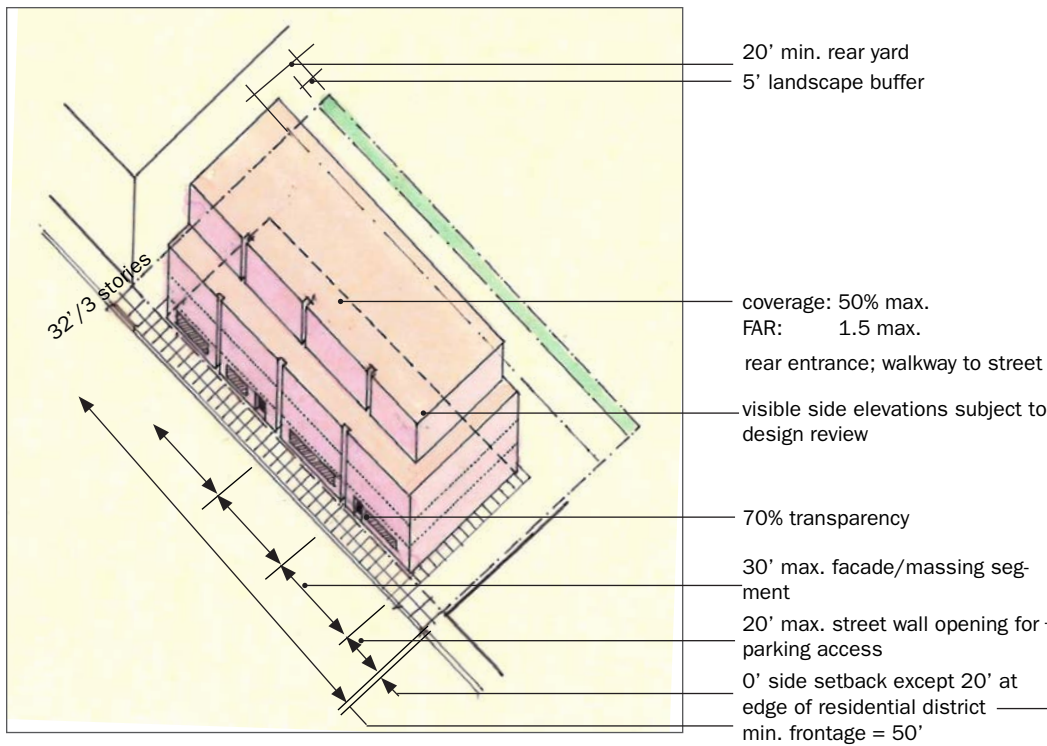


Fig. 6.1: Summary diagram for typical intermediate infill site (between 10,000 sf and 20,000 sf.).

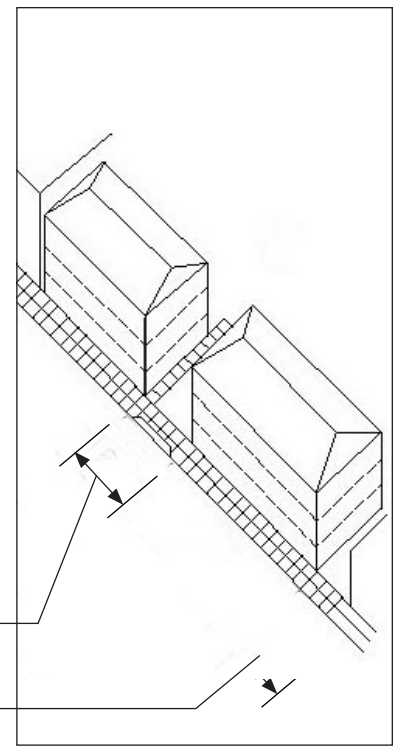


Fig. 6.2: Frontage with two structures (see also fig. 7.1)

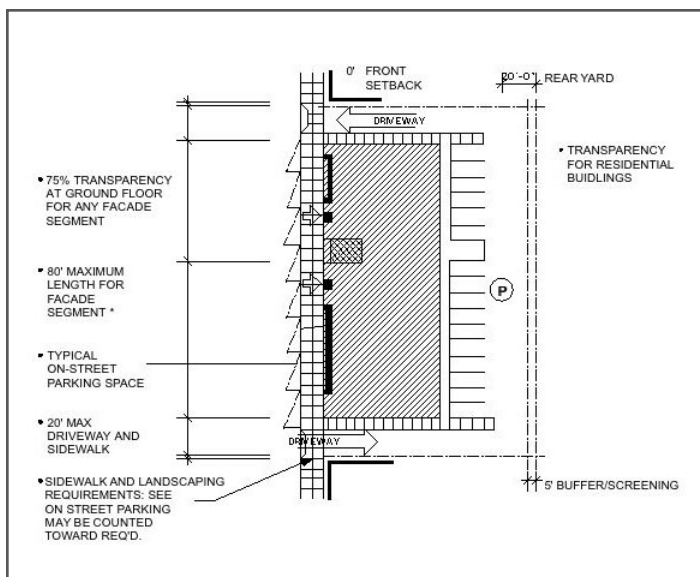


Fig. 6.3: Summary diagram for typical intermediate infill site with long frontage.

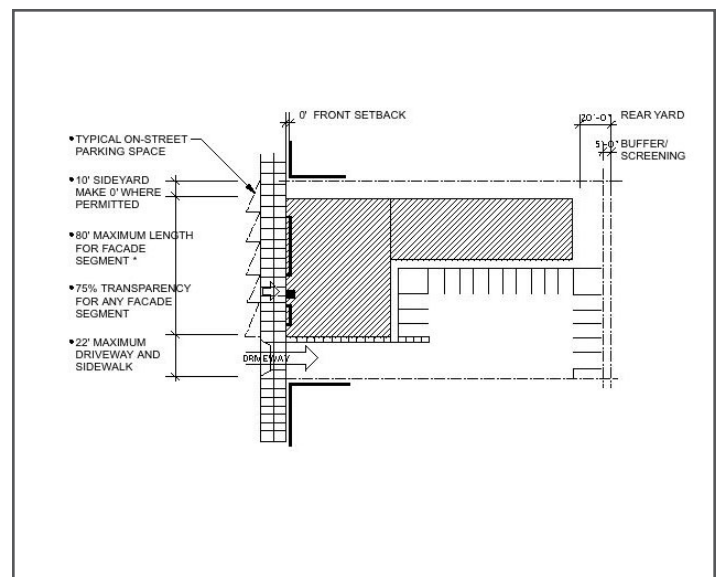


Fig. 6.4: Summary diagram for typical intermediate infill site with narrow frontage and deep lot.

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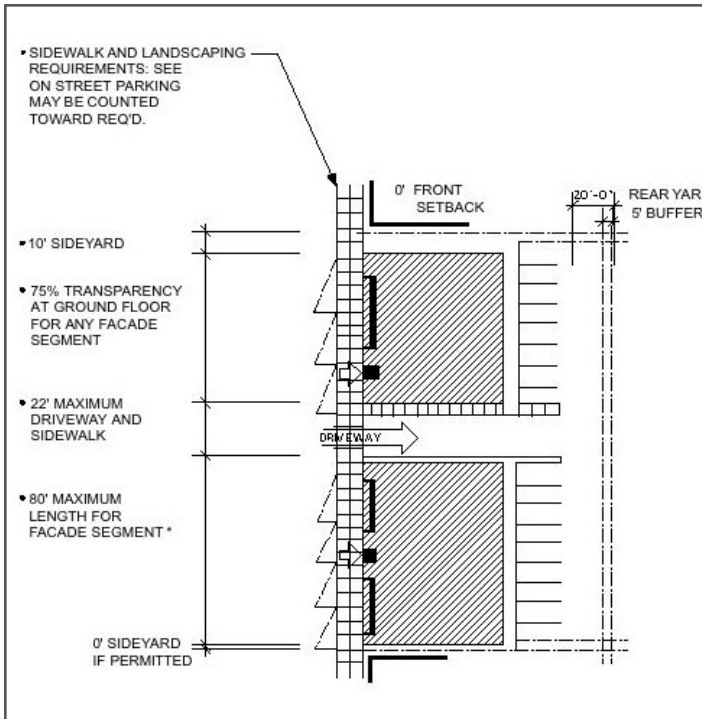


Fig. 7.1: Site plan for intermediate infill site with long frontage and two separate structures.

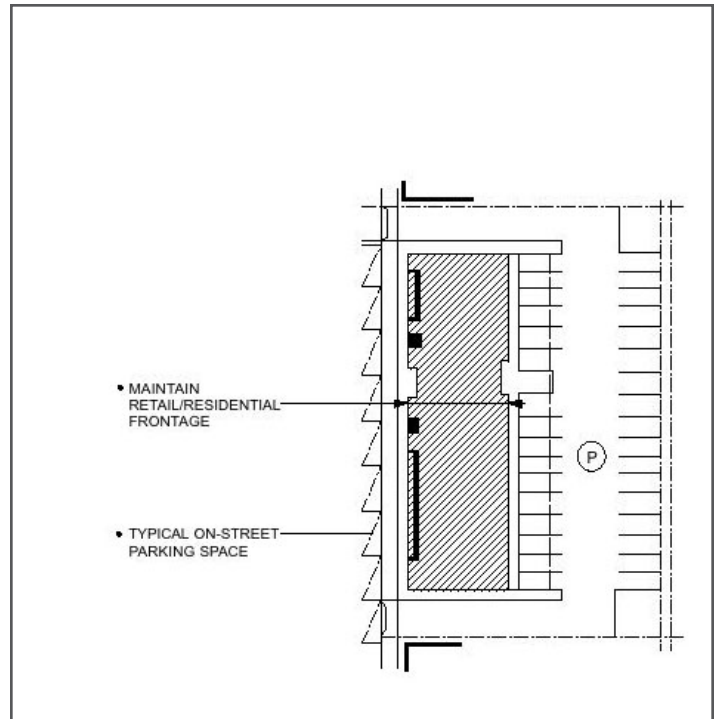


Fig. 7.2: Site plan for intermediate infill site with surface parking lot partially beneath building. (See also photos, Figures 7.3 and 7.4.)



Fig. 7.3 and Fig. 7.4: Photo of infill building with strong pedestrian orientation to the street and surface parking that is partially beneath the building and with access to the parking lot from the side street.

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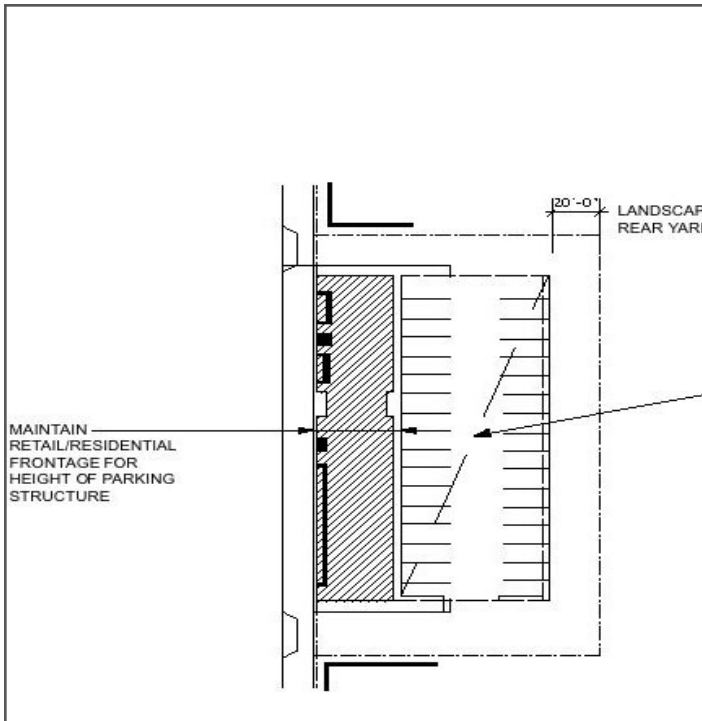


Fig. 8.1: Site plan for intermediate infill site with integrated parking structure. See also photo, **Figure 8.3**

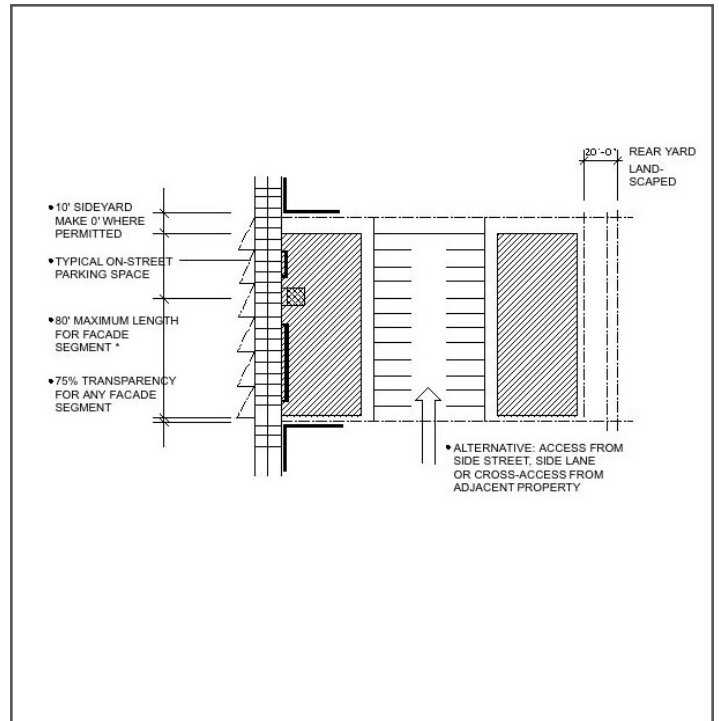


Fig. 8.2: Site plan for intermediate infill site with narrow frontage and deep lot showing alternative access to parking.



Fig. 8.3: These street-oriented mixed-use buildings obscure the parking structure behind.

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Lots greater than 20,000 sq.ft.

Discretionary review the Zoning Board shall have discretion during site plan approval to allow for superior design and flexibility in site regulations in order to advance community priorities and provide appropriate amenities. Streetscape elements, landscaping, public space, and other public amenities shall be considered. (See Architectural criteria and design standard section)

Commercial and residential buildings shall be sited to provide functional, livable outdoor spaces, and public spaces which enhance the use of the building and, to the greatest extent possible, the neighboring buildings.

In general, lot coverage of large scale buildings should be 50% and buildings should adhere to the following recommended regulations:

- a. Minimum frontage: 50 feet
- b. Minimum setbacks:
 - Front yards: 0 feet
 - Rear: 20 feet
 - Side: 0 feet, but at least 4 feet if provided
- c. Maximum Building Height: 3 stories and 32 feet to setback and 4 stories and 45 feet to uppermost roof or parapet, as measured from the average established grade of the curb of adjoining streets, provided that no less than 50% of the perimeter of the property directly abuts public streets. Where less than 50% directly abuts public streets, building height shall be measured from the average level of the finished ground surface adjacent to the exterior walls of the building. (Fig. 5.1)
- d. Maximum Building Coverage: 30%, provided that canopies and other roof coverings over walkways and streets shall not be included, and also parking structures, suitably screened and landscaped, with roof or upper-deck elevation not exceeding five (5) feet above average street grade shall not be included in the calculation of building coverage.
- e. Maximum Floor Area: total floor area (exclusive of parking floor) shall not exceed the amount of 1.0 floor area.

For the three categories of sites the following is recommended:

- The following areas shall not count against the base or maximum FARs described:
 - (1) Floor area of basements occupied by parking.
 - (2) Floor area of plazas and other passageways through buildings.
 - (3) Others (add as appropriate).
- FAR bonuses are earned in exchange for provision of certain public amenities. Please see Architectural Criteria and Design Standards section for list of bonus-able public amenities

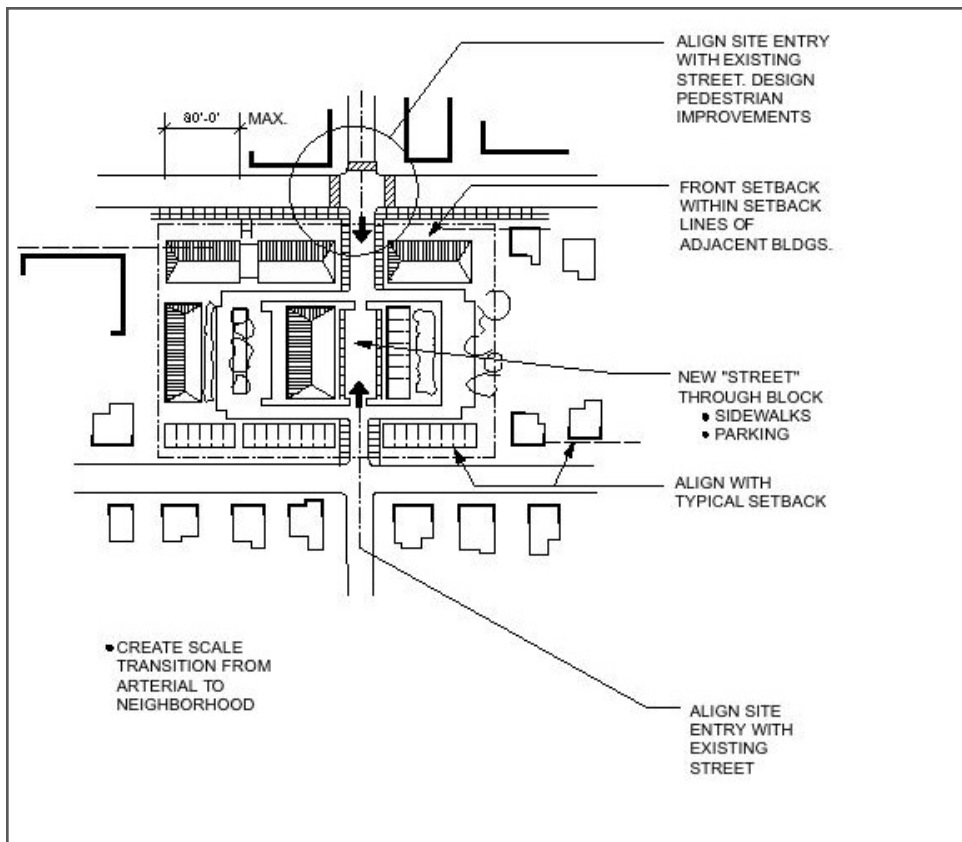


Fig. 10.1: Site plan for larger site development.

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Site Design and Architectural Standards. For sites within the Village Commercial District, site and architectural plans shall conform to the application requirements and review standards of Section 7.2 of these Regulations and to the following additional standards and criteria:

6. Overall Site and Context Considerations:

- a. If a TOD District is adjacent to a Residence District or a residential use, all facilities and buildings shall be screened by appropriate fences, walls and landscaping treatment. Subject to determination by the Zoning Board, all new buildings, exclusive of parking structures, may be required to be set back from any property line which is bounded in its entirety by residentially zoned property in order to provide an appropriate transition and buffer. Development in the TOD district will not be dictated by the bulk regulations of the adjacent residential district: rear yard and landscaped buffer regulations will provide the necessary transition.
- b. Roof structures, machinery, and mechanical equipment, such as equipment for heating ventilation and air condition, shall be integrated into the design of the buildings. Any rooftop mechanical equipment shall be set back at least 10 feet from upper-level building facades, and set far enough back that it cannot be seen from the street. If visibility from the street is unavoidable, rooftop equipment shall be screened from view in a manner consistent with the architectural design and materials of the building.
- c. All sidewalks and walkways should be provided with lighting averaging at least one-half (0.5) foot candle across the area to be lit. Lighting fixtures should be building-mounted above sidewalks or located in bollards along the walkway. All fixtures must be shielded. All site lighting shall be directed onto the site and shall be shielded from adjacent residential uses or zones and from the adjoining street.
- d. Front setbacks are not permitted so as to create a continuous building wall along the sidewalk or to align with adjacent property frontage. Openings in the wall are only permitted for driveways (Fig. 4.1, 6.1). See below for standards.

The Director of Planning may approve variations from the setback limit under the following circumstances:

- ➔ A building may be set back to accommodate irregular frontage condition of existing adjacent buildings. (Fig. 11.1)
- ➔ A building façade may, in some instances, be built to the minimum setback of existing buildings on adjacent properties in order to maintain a consistent street edge.
- ➔ A building façade may, in some instances, be set back farther from the front property line due to site constraints such as existing landscape features or inadequate sidewalk width.

- e. Require buildings to be oriented to the street. Require buildings in commercial areas to be constructed up to the front property line. (Figs. 4.1 and 6.1)

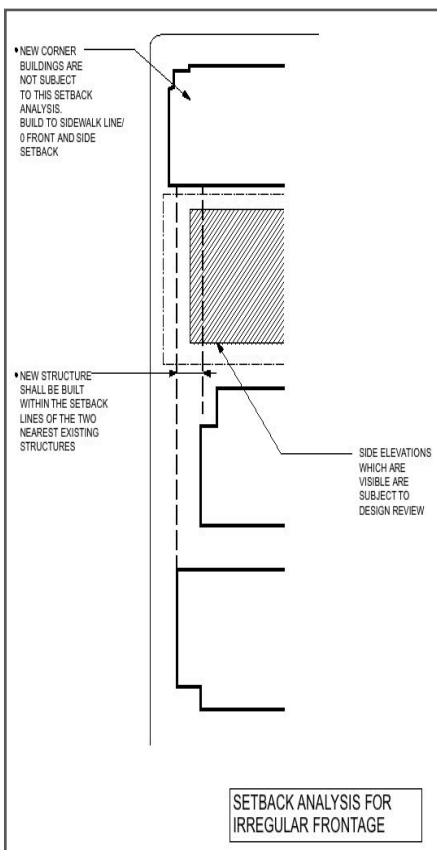


Fig. 11.1: Set back analysis for irregular frontage.

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- f. The principal building entrance and front shall face the street frontage and sidewalk. The main entrance shall not be oriented toward a parking lot. Where parking is located to the rear of a building, a secondary entrance should be provided for direct access to the parking area or a walkway should lead to the primary entrance at the street. All entries should be well lit and should include architectural treatment that heightens their visibility.
- g. Buildings on a corner lot or a lot fronting on two streets shall have the main entrance on the primary street. This requirement does not preclude additional rear or side entrances facing parking areas. (Fig. 12.1, 12.2, 12.3)
- h. Service areas, trash and/or dumpster areas shall be located away from facades and away from streets, screened by wood fences or landscaping, or a combination of both. (screening).
- i. Buffer planting shall be provided between commercial businesses and adjoining residential uses.
- j. Deliveries and loading activities restricted to weekday/daytime hours.

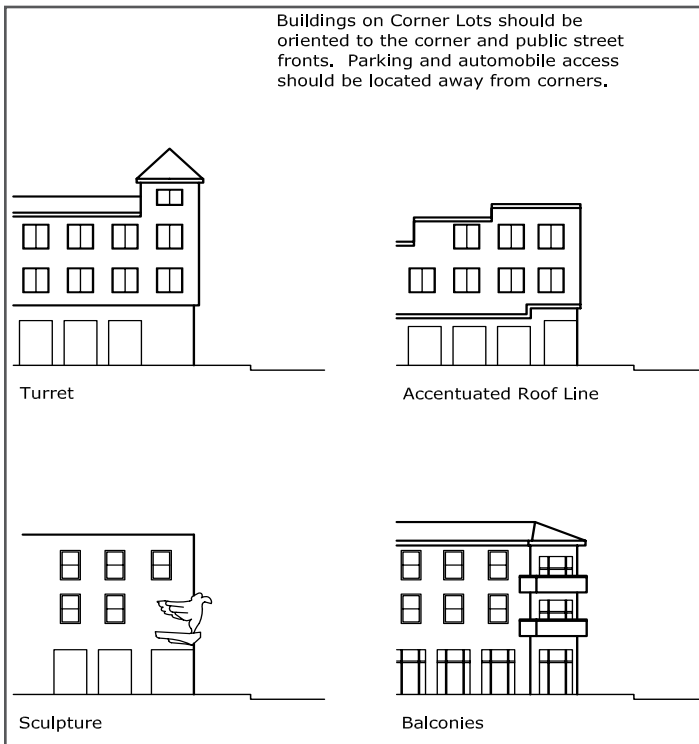


Fig. 12.1: Strategies for articulating the corner of a building with two frontages.



Fig. 12.3: The massing of a building should reflect the importance of a corner site and relate to both street frontages.

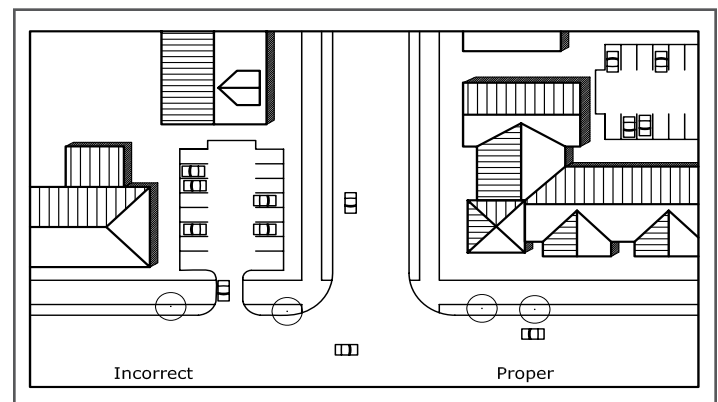


Fig. 12.2: Parking and automobile access should be located away from corners.

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7. Building Design Standards:

(see also Design Guidelines, Pages 20 thru 27)

- a. At least 75 percent of linear width of the front façade shall be comprised of transparent windows. Where shade is desired, awnings are encouraged. All windows shall be at least 6' in height with the window sill no more than 3' above grade. Where storefronts do not open to the street, building facades should be highly articulated with projections, recessions, windows, and other design elements to avoid blank, featureless areas. No more than ten linear feet of blank wall should be permitted, except in the case of decorative walls screening parking.
- b. Windows on upper floors should be residentially-scaled double-hung, casement or other multi-paned styles. Fixed glass windows should not be used on the upper levels, nor shall upper-story windows be painted over or otherwise obscured. (Fig. 4.1, 6.1)
- c. Building materials should include stone, brick, wood clapboard, cementitious stucco with half timber or wood shingles. Vinyl and aluminum siding are not appropriate materials in the downtown districts. Buildings should have varied roof lines and materials. Peaked, mansard and other sloping roof types are encouraged. Flat roofs should be topped with cornices or decorative parapets.
- d. Signs should be integrated into the façade, typically located in a sign band above the storefront windows. Wooden signs are strongly encouraged, while citra board and corrugated plastic signs are prohibited. Signs should be externally lit. Signage shall conform to the standards of the ARD District, except that ground sign and pole signs are not permitted. (Section 7.6)
- e. Security gates shall be integrated into the design of the facade. No security gate mounted flush to the facade shall be permitted. Security gates shall be a minimum of seventy-five percent (75%) open for the upper seventy-five percent of the (75%) of the gate and in no instance shall any solid portion of the gate be higher than forty-eight (48) inches from the adjoining sidewalk.
- f. Facades shall be broken up into lengths of no more than 30 feet, through use of variations in façade plane, piers, or other architectural features.
- g. Fencing materials should be of tubular steel or wrought-iron-type milled steel picket along public rights-of-way. Fencing along side or rear yards, or within a lot, may be wood, steel pickets or any other approved fence type. No chain link fencing should be permitted anywhere within the downtown districts, except during construction.

8. Parking Requirements:

Clarify further the breakdown of parking spaces by term and user: provide short-term parking in front of uses with high turnover, medium term parking for the rest of on-street spaces and in the most convenient parts of off-street lots and long-term parking elsewhere.

The standards of Section 12 apply when the TOD zone does not establish a separate parking standard.

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- a. The number of off-street parking spaces in the TOD District shall be provided in accordance with the following regulations.
 - ➔ For every 1 unit created, 1.5 residential dedicated parking spaces will be provided
 - ➔ For every 1,000 sq. ft of local retail 3 parking spaces will be provided
 - ➔ Parking spaces within 400 foot radius of site. In residential buildings with 1 car per unit, leased parking spaces can be located within 1,000 foot radius of the site
- b. Per parcel, waive parking requirements for new retail businesses occupying 3,500 sq. ft. or less. When there are multiple retail spaces in a development, the waiver only applies to the first retail tenant.
- c. Waive parking requirements for new construction and renovations within 500 feet of a dedicated parking garage or parking lot.
- d. Parking may also be located fully below buildings, partially below grade in a building, or at-grade within a building, provided it is fully enclosed and no entry is provided facing a public street or front yard. Structured parking that is partially below grade shall be screened from the street by steps, trellises, or screens. (Fig. 7.3, 7.4, 8.3)
- e. Require public and private parking lots to provide landscaping both throughout the parking lots as well as on their perimeters. In order to improve the appearance of parking lots, large areas of surface parking should be broken up by rows of landscaping no less than 10 feet in width, in order to create parking “fields” of no more than 50 spaces each. Landscaping shall include ground cover, ornamental grasses, or low shrubs.
- f. For shop fronts and workplaces, on-street parking directly affronting a lot shall count toward fulfilling the parking requirement.
- g. No surface parking can be located on street corners of lots. (fig. 12.2)
- h. Parking lots, approved as a conditional use, shall meet the following design requirements: (Fig. 16.1, 16.2, 16.3, 16.4)
 - 1. Be in the rear and/or interior side of buildings; no surface parking for motor vehicles is allowed between the front property line and the adjacent building;
 - 2. Be even with or behind building façades;
 - 3. Extend no more than XX feet along any street frontage, without a vertically prominent and active use interrupting the parking streetscape;
 - 4. Be screened from any adjacent street by a wall 3 to 4 feet in height; alternatively, landscaping 3 to 4 feet high may be used if it screens parking with a least 75 percent opacity; (Fig. 16.1, 16.2, 16.3)
- i. All loading and service areas, trash receptacles and mechanical equipment should be screened by means of solid fencing supplemented with evergreen plantings. Plantings should be maintained at a minimum height of four feet.

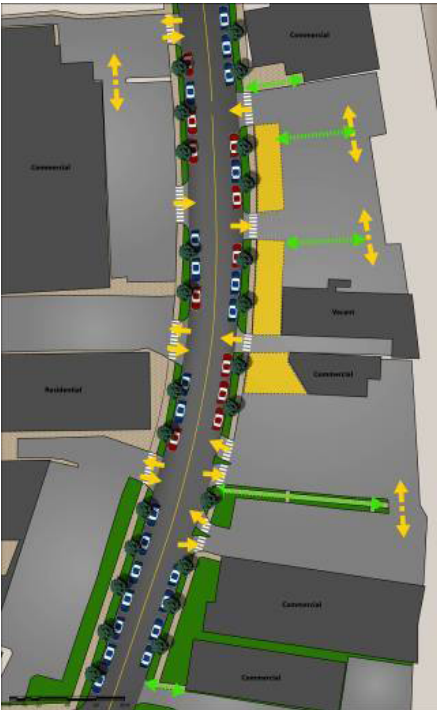


Fig. 15.2 Additional on-street parking through streetscape and access management.

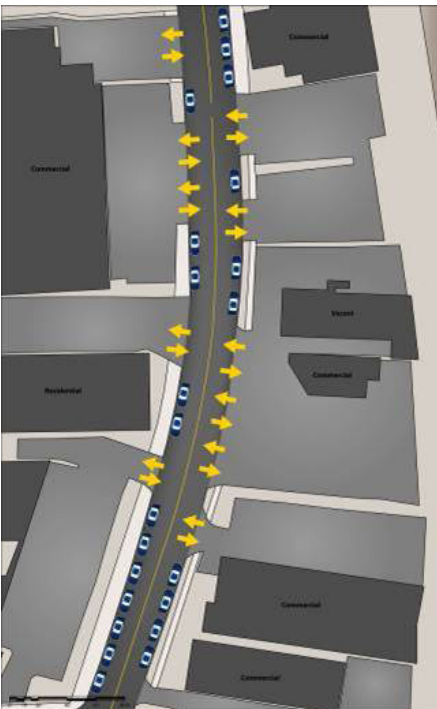


Fig. 15.1 Existing inefficient parking and access conditions on a typical commercial street.

9. Curb Cuts

- If a curb cut is needed, pedestrian safety can be enhanced by reducing the width of the curb cut or by consolidating driveways. In most cases, a single lane is sufficient to serve several apartments or commercial spaces.
- Curb cuts to parking lots and garages shall be minimized by sharing driveways for access to adjacent parking lots. However, two curb cuts and driveways are prohibited along the front property line for properties less than thirty (30) feet in width. The number of vehicle entrances should be minimized through the use of shared parking facilities. The width of vehicle entrances should be minimized and in no case should they exceed 24 feet. Curb cuts should be located directly opposite one another where possible. Redundant curb cuts should be eliminated when possible. (Fig. 15.1, 15.2)
- Curb cuts, driveways, and garages shall meet the following dimensional regulations:
Curb cuts for parking lots of ten (10) or fewer spaces shall not exceed ten (10) feet in width along any point; of eleven (11) or more spaces shall not exceed fourteen (14) feet in width at any point.

10. Shared Parking

Wherever feasible, shared parking or the use of public parking lots is encouraged.

- Shared parking shall be permitted on individual parcels (mixed use) as well between two or more parcels, where the peak parking of two or more uses occurs at different times.
- Uses sharing a parking facility do not need to be contained on the same lot, but each use shall be a maximum of 500 feet (or 800 ft) from the closest parking space in the lot providing the shared spaces.
- Appropriate signage must be provided to clearly identify proper users (i.e. “Customers” vs “Visitors”)
- A written agreement, covenant, deed restriction or other document as determined necessary by the hearing authority shall be executed by all parties to assure the continued availability of the shared parking spaces for the life of the proposed development or use.
- Where a finding is made by the Zoning Board that individual uses such as theater, general office and others will experience peak parking demand at different times than the peak retail parking demand, the Zoning Board may authorize a reduction in parking by recognizing the opportunity for such uses to share common parking spaces. The general methodology entitled “Shared Parking,” published by the Urban Land Institute in 1983 as amended, may be used to determine such parking reductions, with additional consideration given to established patterns of uses of individual establishments.
- Adjacent parking lots shall have vehicular connections via an alley or internally. On-street parking directly in front of the property shall count toward fulfilling the parking requirement of that lot.

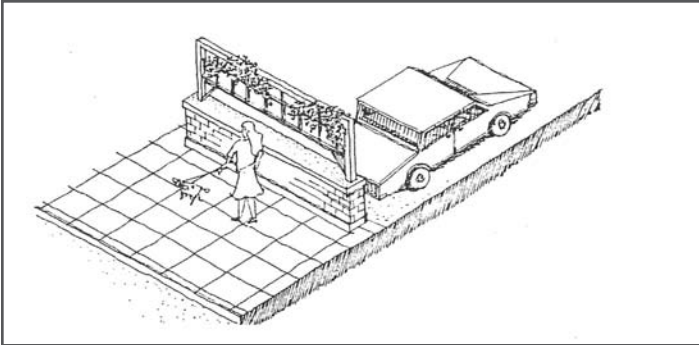


Fig. 16.1: Surface parking screened by an architectural wall and trellis.

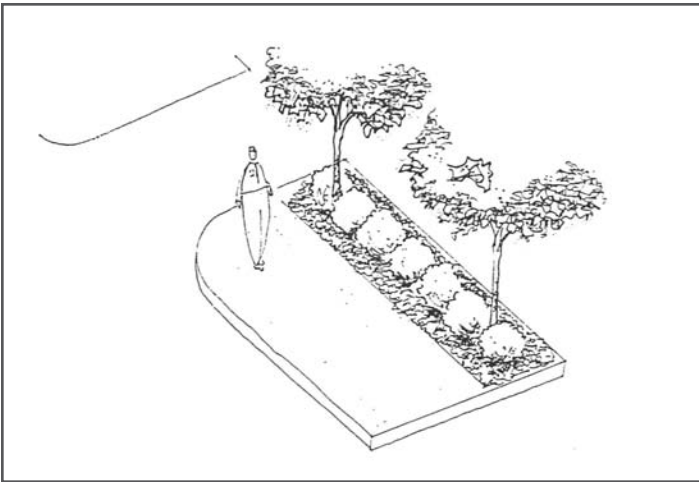
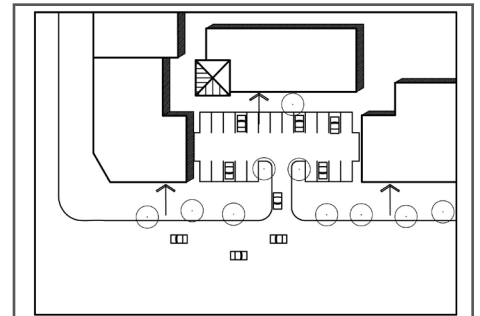


Fig. 16.2: Surface parking screened by hedges and trees

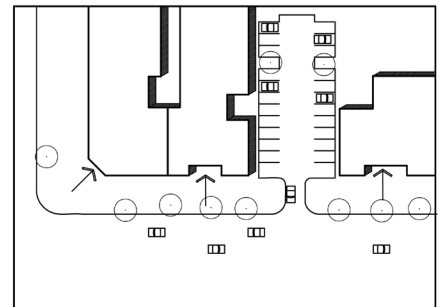


Fig. 16.3: Photo of surface parking screened by hedges and trees.

Not acceptable:
large lot between
the street and the
storefront



Acceptable:
Limited storefront
parking lot



Preferred:
Parking behind
retail frontage

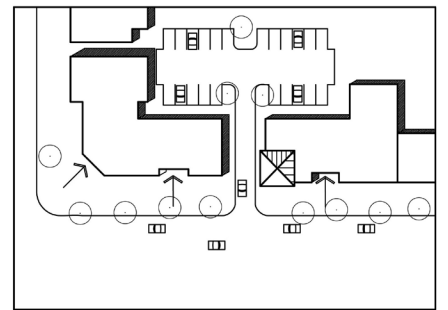


Fig. 16.4: Acceptable and unacceptable parking configuration.

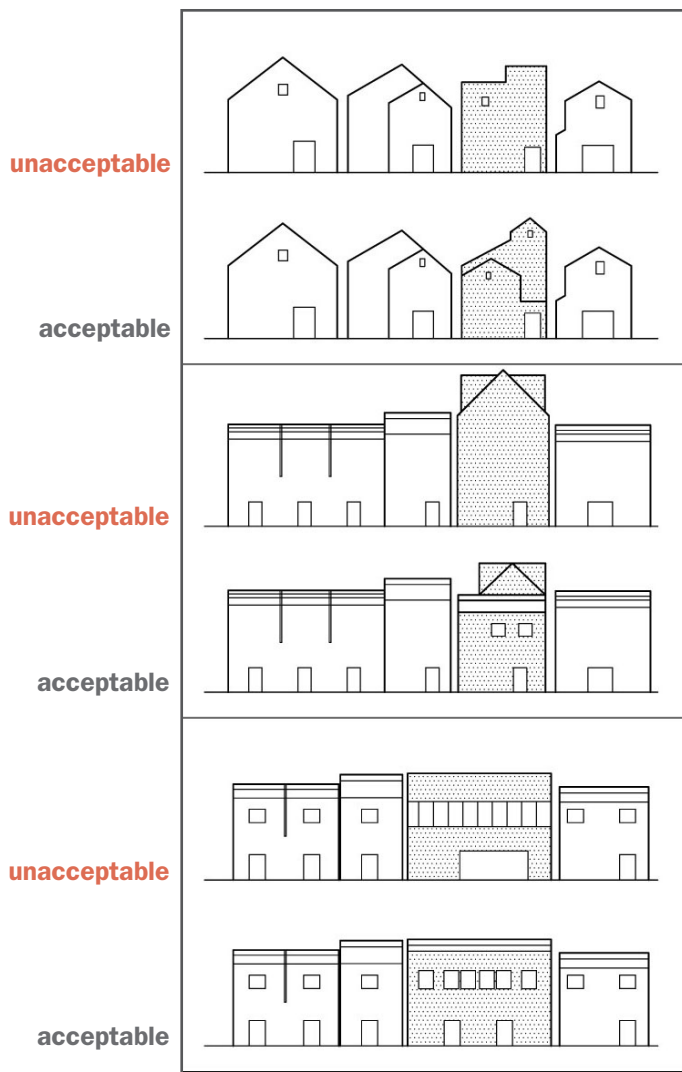
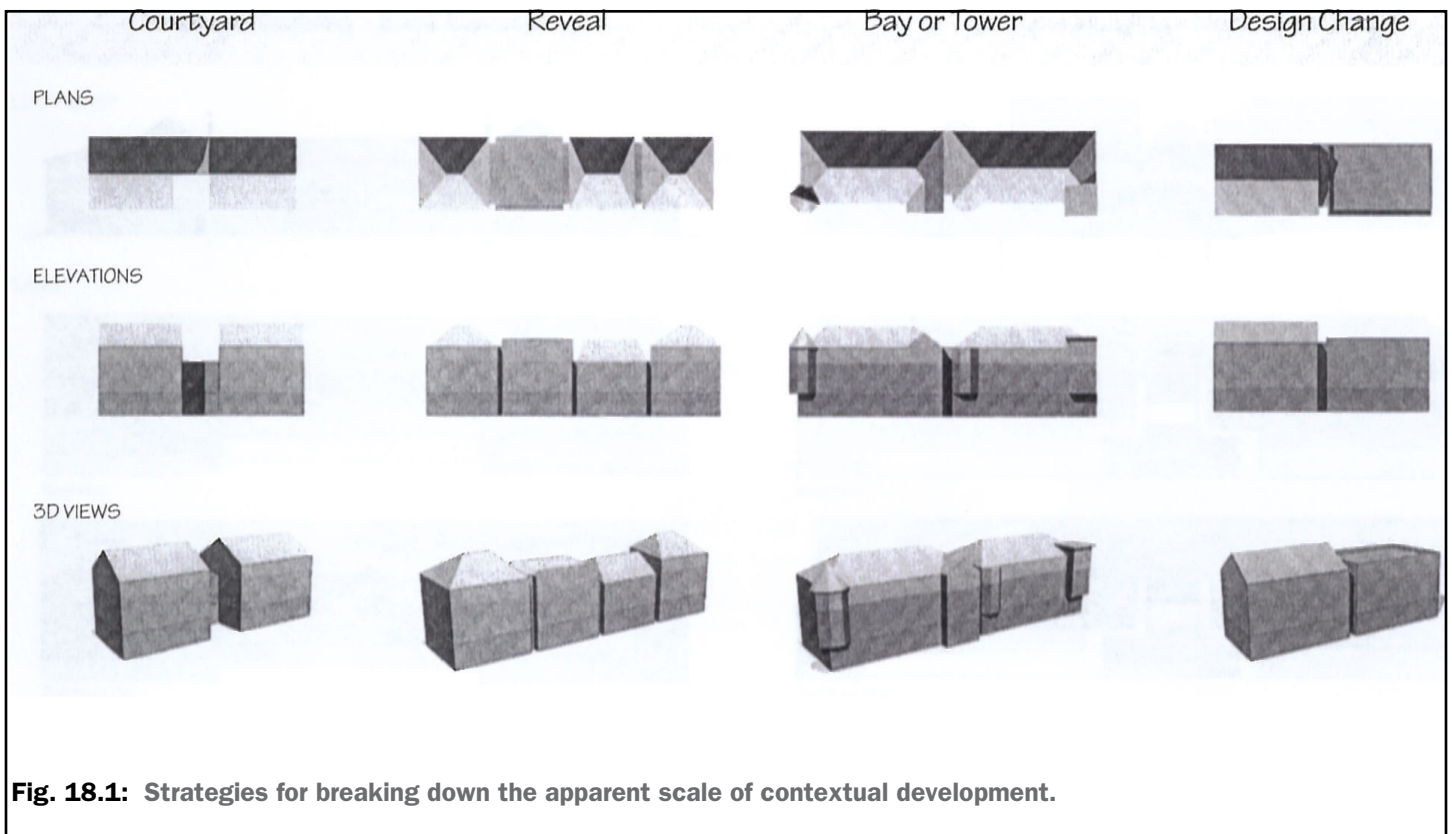
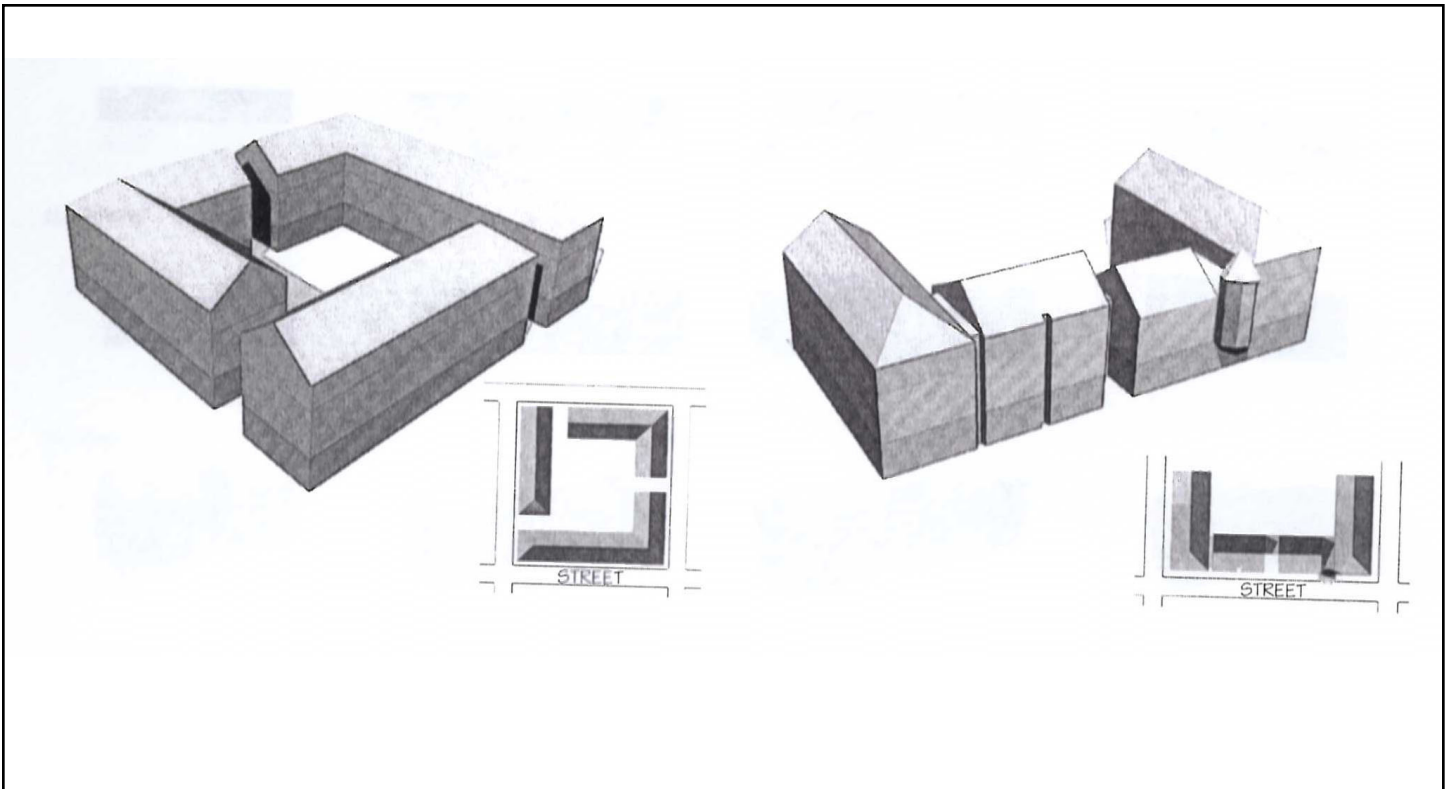


Fig. 17.1: Acceptable and unacceptable façade designs for infill contexts in terms of a) roof form; b) massing and; c) window opening types and rhythm.

11. Overall site and Context Guidelines

- a. Projects should be compatible with the scale of development anticipated by the applicable Land Use Policies for the surrounding area and should be sited and designed to provide a sensitive transition to nearby, less intensive zones. Projects on zone edges should be developed in a manner that creates a step in perceived height, bulk and scale between the anticipated development potential of the adjacent zones.
- b. Efforts to coordinate the actual and apparent height of adjacent structures are encouraged. This is especially applicable where buildings are located very close to each other. It is often possible to adjust the height of a wall, cornice or parapet line to match that of an adjacent building. Similar design linkages can be achieved to adjust apparent height by placing window lines, belt courses, and other horizontal elements in a pattern that reflects the same elements on neighboring buildings. (Fig. 17.1)
- c. Structures should be designed to reduce their perceived height and bulk by dividing the building mass into smaller-scale components. On larger buildings, the rooflines of buildings shall follow the variation in bay massing so as to appear as a series of side-by-side buildings or of bays. Rooflines should be emphasized, for example with gabled or other pitched roof forms, parapets, balustrades, and/or cornices. (Fig. 18.1)
- d. In areas where there are changes in land use or residential density, new residential development shall be designed to provide a transition between uses (through the use of setbacks, site plan, building massing, drive-ways locations, etc.).
- e. New multiple family residential development shall respect the scale and character of the adjacent residential neighborhood through attention to views, building scale and orientation, proximity to adjacent uses, location of driveways, noise, lighting and landscape.
- f. Multiple buildings on the same site shall be designed to create a cohesive visual relationship between the buildings. (Fig. 10.1)
- g. The screening wall or landscaping should be compatible with adjacent structure and with existing building materials.

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12. Building Design Guidelines:

(see Building Character Guidelines pages 24 and 25)

- a. New or additional buildings shall be coordinated with the existing context by use of colors, materials and linear emphasis so as to constitute a single, coordinated whole recognizable as a unit.
- b. Architectural design shall be compatible with the developing character of the neighboring area. Design compatibility includes complementary building style, form, size, color and materials. Echo the prevalent character of surrounding buildings on the block, while allowing for freedom of architectural style.
- c. Buildings should be designed to have a clear base, middle and top, with horizontal elements separating each. (Fig. 20.2, 20.3)
- d. Electronic communication equipment should be of materials and colors similar to the surfaces upon which they are mounted, or otherwise screened from view.
- e. Where larger scale retail stores with building widths or frontages exceeding 25 feet are planned, architectural details should be used to create the appearance of multiple storefronts.
- f. A parking garage shall be architecturally compatible with the principal building and all exposed exterior walls shall be faced with finished materials, such as brick or masonry. No more than 50% of any exterior facades shall be open and a solid wall of not less than three feet in height above each floor level along the exterior of each level if the garage shall be provided. The provision of ground-floor retail space within garages that face the street shall be strongly encouraged.
- g. Building facades should be articulated by using color, arrangement, or change in materials to emphasize the façade elements. The planes of the exterior walls may be varied in height, depth or direction. Extremely long facades shall be designed with sufficient building articulation, reveals and, in some cases, landscaping to avoid a monotonous or overpowering institutional appearance.
- h. Any changes in primary wall material from lower to upper levels should occur along a horizontal line, with the visually-heavier material below the visually-lighter material. Paneling materials applied to one façade only, such as brick paneling, should be extended around building corners to a logical break in plane, so as to look substantial rather than “pasted-on.” Facades should be designed so that any seams or expansion joints are rationalized by the logic of the composition.

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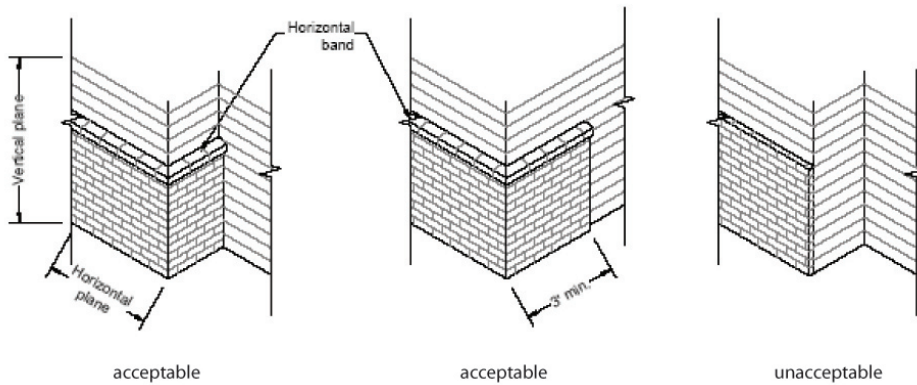


Fig. 20.1: Treatment of materials at corners.

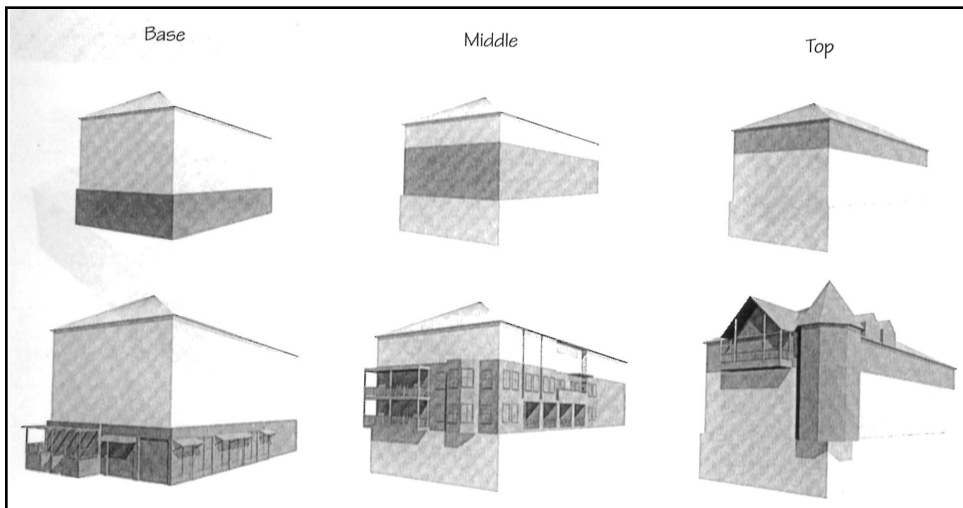


Fig.20.2: Strategies for articulating the massing and façade of a building to express a base, middle and top.



Fig. 20.3: Photo of contextual elevations which express a base, middle and top.



Fig. 22.1: Unacceptable: Photo simulation of unacceptable residential development. This building has no orientation to the sidewalk and half of the frontage is a pedestrian un-friendly surface parking lot.



Fig. 22.3: Preferred: Photosimulation showing the kind of pedestrian-oriented mixed-use building enabled by TOD zoning.

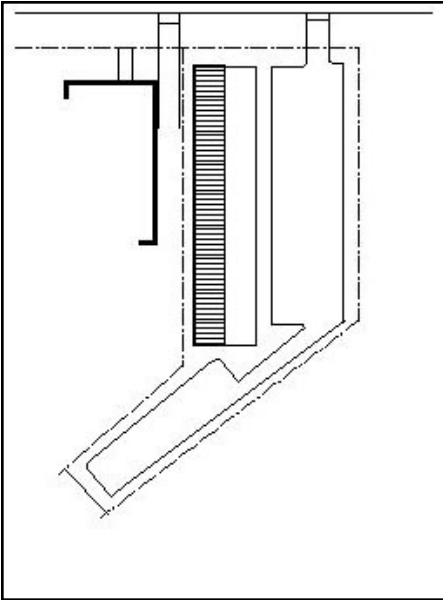


Fig. 22.2: Unacceptable: Site plan of typical deep-lot residential development with no orientation to the street.

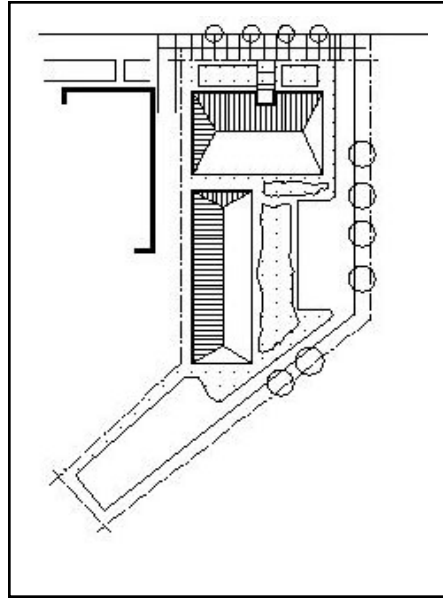


Fig. 22.4: Preferred: Site plan of preferred configuration for new mixed-use residential development enabled by TOD zoning. Except for the narrow driveway, the building frontage maintains the definition of the street.



Fig. 23.1: Unacceptable: Photosimulation of unacceptable commercial development. This building has no relationship to the sidewalk and forces pedestrians to walk by a surface parking lot.



Fig. 23.3: Preferred: Photosimulation of preferred configuration for a pedestrian- and sidewalk-oriented commercial development.

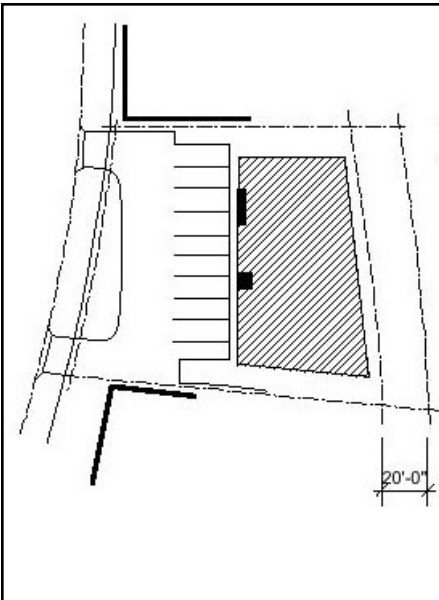


Fig. 23.2: Unacceptable: Site plan for a typical unacceptable commercial development that has no relationship to the sidewalk or street.

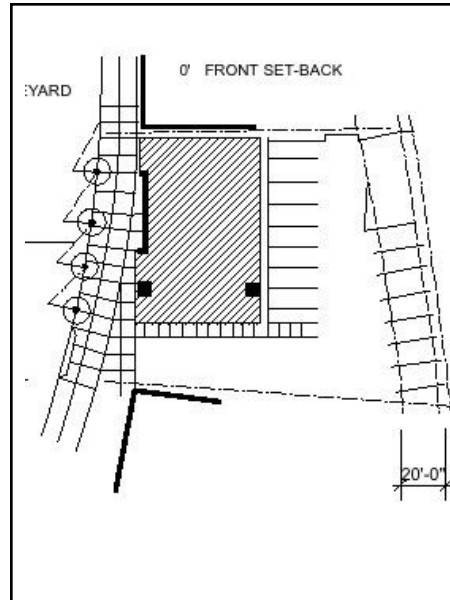


Fig. 23.4: Preferred: Site plan showing the preferred configuration for contextual infill commercial development with a pedestrian-scaled façade on the street and parking behind.

Design Guidelines for a Village Scale TOD

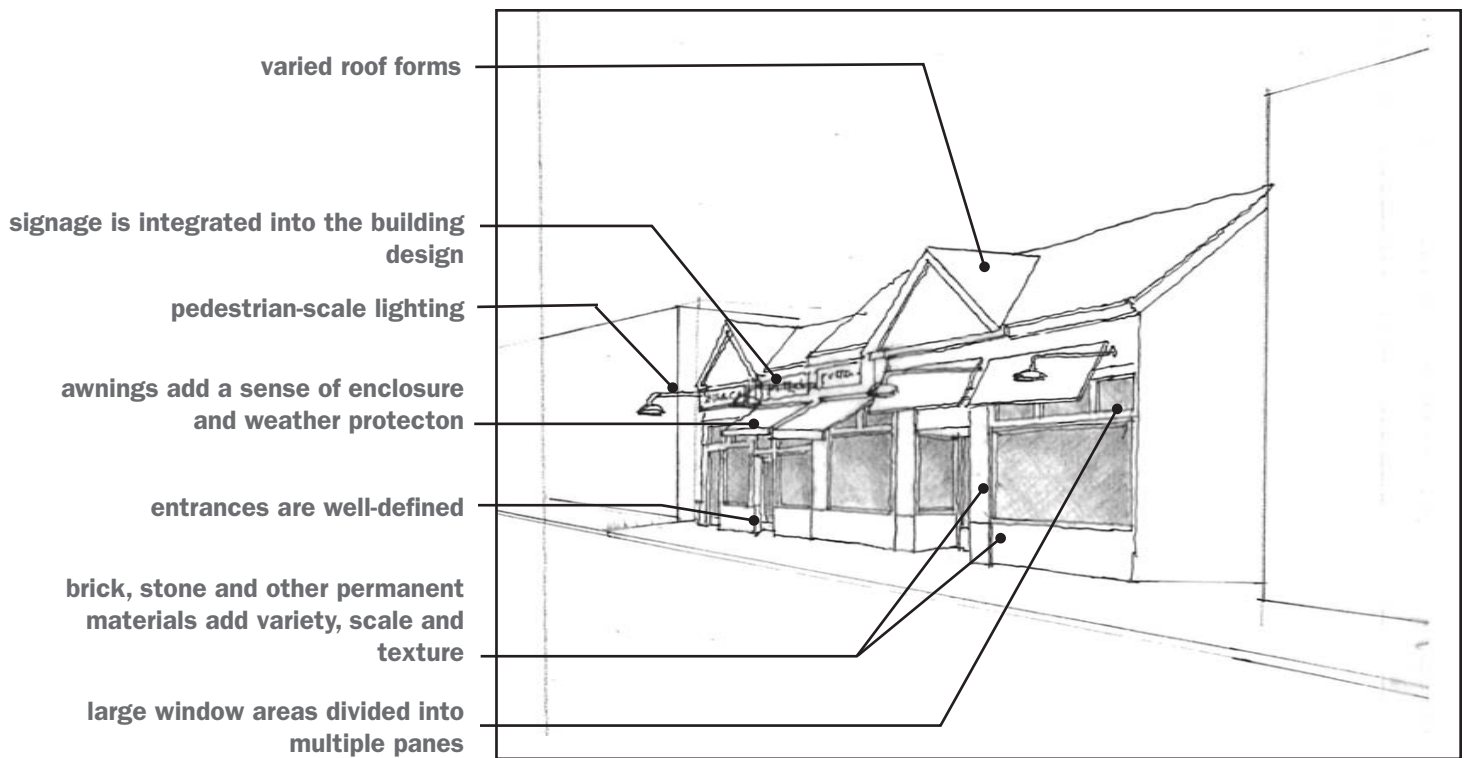


Fig. 24.1: Character guidelines for 1-story commercial infill building



Not Acceptable: This building has no orientation to the street



Not Acceptable: These buildings have no orientation to the street; driveway is excessively wide; parking is inadequately screened.



Preferred: These buildings are oriented toward the street and respect neighborhood context.

Design Guidelines for a Village Scale TOD

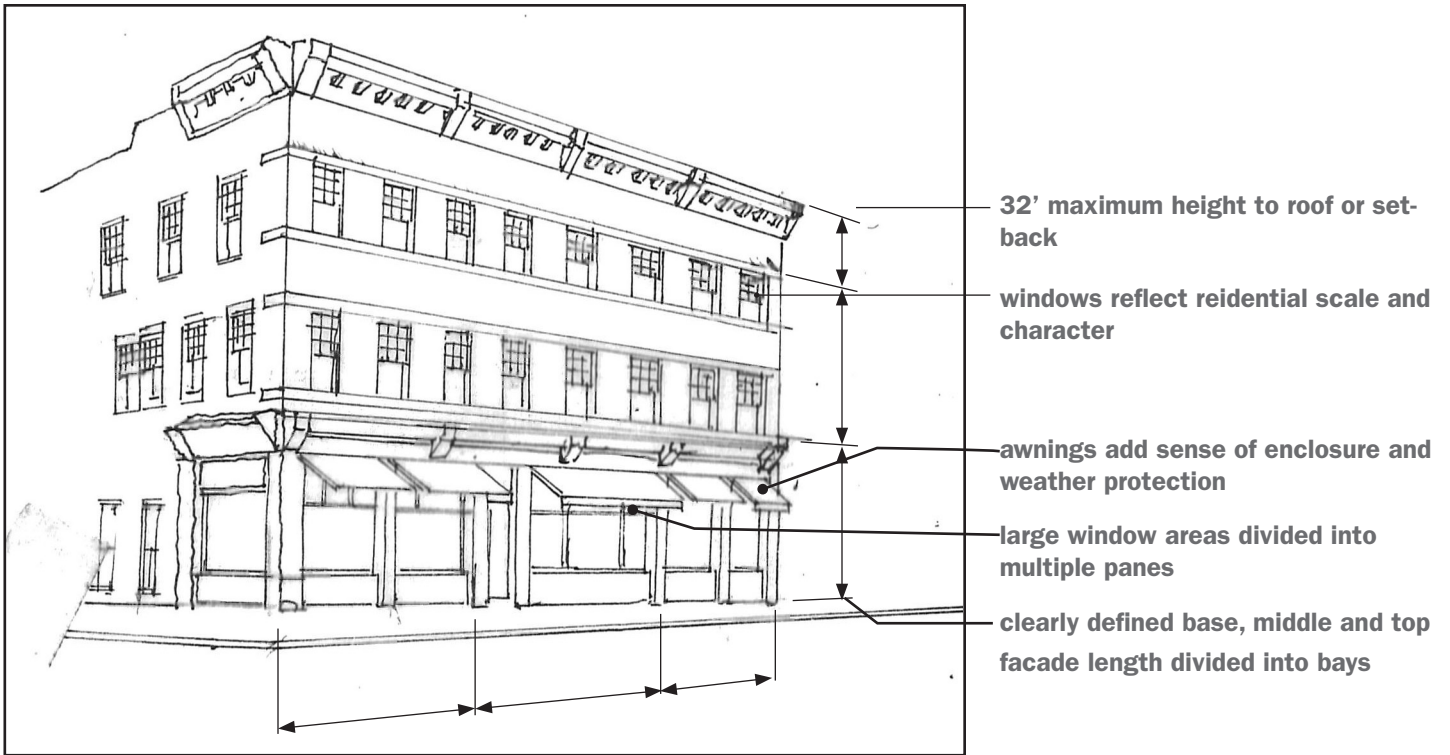


Fig.25.1 : Character guidelines for 3-story mixed-use infill building



Preferred: These buildings present a "friendly face" to the street



Not Acceptable: No significant changes in massing; no set-back at 3rd floor; no entrances on the main street; no response to the corner.



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