



Annual Report FY 2019



The Hudson
River tunnel
to Penn Station
was completed in

1910

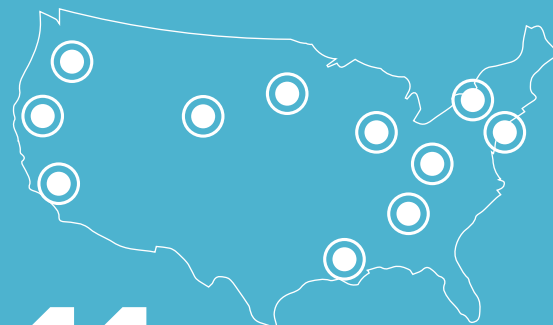


Find out about Gateway on page 7

23 years

since RPA first recom-
mended congestion
pricing in Manhattan

Read about our win on page 4



11 regions

from around the country participated in our
Planning Exchange

Learn more on page 8

10 recommendations

to restore public housing for more than 400,000 New Yorkers

The time to act is now — read more on page 5



partner organiza-
tions working on
affordable housing
in Fairfield County

Learn more on page 11

1100

MegaWatts of
energy from
wind turbines
being devel-
oped in NJ

More on NJ on page 9



350

community mem-
bers in Central
Queens complet-
ed the climate
resilience survey

RPA created with MRNY's core team of com-
munity activists, BASTA!

Learn more on page 10

Co-Op City

Jackson
Heights

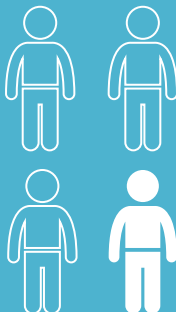
Brooklyn
Army
Terminal



100,000 daily commuters

would travel on the Triboro Line starting on day one

The tracks are already there, let's put them to use! More on page 6



3 out of 4 children

— most from communities of
high poverty, crime, and poor
health — cannot walk to a
safe park

Discover New City Parks on page 12

31 counties
783 cities and towns
23 million residents
1 Regional Plan Association



Learn more
at rpa.org!

To our RPA supporters,

What an exciting year it has been!

In just the past twelve months:

- ▶ New York became the first state in the nation to approve a congestion pricing program and used it to enact a record-setting \$51.5 billion MTA capital plan.
- ▶ New York also passed one of the most ambitious climate initiatives, committing the state to 100% carbon-free electricity by 2040 and economy-wide carbon reductions of 85% by 2050. Collectively our three states committed to develop 18.5 GW of offshore wind capacity — enough to power more than 10 million homes with clean, renewable energy.
- ▶ And the Port Authority of New York and New Jersey adopted ambitious plans to remake our region's airports and provide better transit access to Newark and LaGuardia.

We were proud to support and advocate for many of these actions, and more.

With your help, RPA has produced more than a dozen reports and hosted countless forums this year, all building on the 61 recommendations in our Fourth Regional Plan. Our research quantified the economic and human costs of shutting down the Northeast Corridor for repairs; we outlined how the rebuilding of the Brooklyn-Queens Expressway can be achieved with fewer lanes of traffic; we proposed innovative ideas to implement congestion pricing in a manner that maximizes revenue for our ailing transit system while reducing traffic; we highlighted the

challenges facing low-income, immigrant, and communities of color because of the growing climate crisis; and we made bold recommendations to make NYCHA the centerpiece solution for New York City's housing plan.

But our work didn't end there. You'll see what else we've accomplished together in the following pages of our Annual Report.

As we turn to an exciting 2020, we face another set of challenges in the region — and nationally — that RPA is uniquely positioned to take on, both in our research and advocacy. How do we reconcile the immediate needs of powering our region with the ambitious goals of decarbonizing our economy? How do we create more affordable housing and protect our public housing as costs continue to skyrocket? What are new and innovative sources of funding for our over-worked transit systems that we should tap into? We look forward to exploring these issues and more.

We celebrate this work — our progress — with you, and thank you, for your endless support, generosity and commitment to making this region a more equitable, healthy, sustainable, and prosperous one.



Scott Rechler
Chairman



Thomas K. Wright
President and CEO



A Historic Win, But the Fight's Not Over

After more than two decades of advocacy, RPA and other transit leaders celebrated the passage of historic congestion pricing legislation in New York. Just months after the bill was passed, RPA became the first organization to release a report about how to implement congestion pricing in NYC.

Featuring 10 recommendations on program design and pricing structure — from implementing two-way tolling to setting metrics for traffic and environmental goals — the report will inform the MTA and a yet-to-be-formed advisory board, and educate the public and elected leaders as we count down to January 2021, the earliest date that the program can begin.

By pricing traffic congestion, New York has an opportunity to unclog traffic, fund and improve public transit, reduce air pollution, and achieve better public health outcomes. These decisions will not only determine the immediate success of the initiative; they'll also provide the opportunity to create a more efficient, equitable and integrated transportation network that improves mobility throughout the metropolitan region — and becomes a model for the nation.

"The passage of congestion pricing was a highlight of my time at RPA — and my career. RPA put so much collective effort into that victory and remains committed to implementing a program that can be a model for other cities. New York should have the world's best urban transportation system, and congestion pricing moves us in that direction!"

— Kate Slevin

Senior Vice President, State Programs & Advocacy





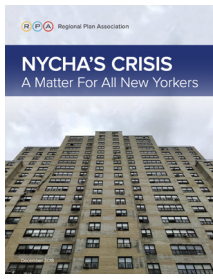
Standing up for affordable housing

If we don't address NYCHA's needs now with a solution that meets the size of the problem, we will rapidly lose our most affordable housing in a city marked by its ongoing affordable housing crisis.

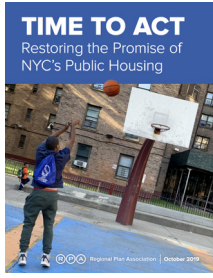
10 actions to repair NYCHA

- 1. Form a new civic coalition for NYCHA
- 2. Make NYCHA the centerpiece of New York City's housing plan
- 3. Create a separate public development entity for NYCHA
- 4. Invest in the next generation of public housing
- 5. Generate new revenue in the long-term by supplementing the existing Transfer to Preserve program with new options
- 6. Have high-end real estate contribute its fair share to NYCHA
- 7. Return to development-based operations
- 8. Create a skills-based exchange program for property managers
- 9. Continue to expand the role of city government in NYCHA operations
- 10. Bring independent voices to the Board

Fixing New York's Public Housing



The rapidly deteriorating state of the New York City Housing Authority (NYCHA), home to more than 400,000 of the city's residents, is no secret. Due to years of neglect and disinvestment, the problem has now reached such a scale — \$45 billion needed in capital repairs — that solving it has become a formidable undertaking. Further deferring needed renovations adds more than \$850 million a year to the ultimate repair bill.



In December 2018, RPA released our first report *NYCHA's Crisis: A Matter for All New Yorkers*, which quantified the impact of allowing NYCHA's "demolition by neglect." We are facing not just the loss of buildings, but also enormous emergency shelter expenses, an economy damaged by a loss of workforce, irrevocably scarred neighborhoods, and massive displacement of thousands of longtime New Yorkers.

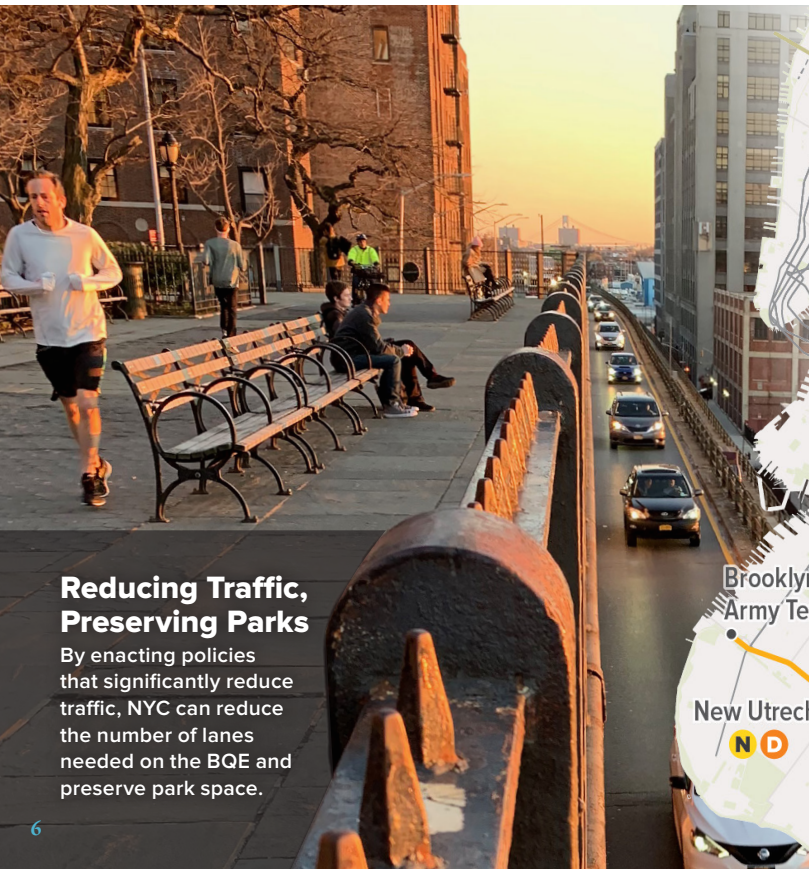
Less than one year later, RPA released a second report, exploring how to bring NYCHA back into a state of good repair. The report highlighted three key actions: an influx of funding, better and more innovative structures for management and capital delivery, and a confluence of major political and civic will.

Redefining Outer Borough Transit

This year, RPA's research and advocacy teams worked to improve transit in New York City's outer boroughs, from reimagining existing infrastructure to expanding public transit options in underserved neighborhoods.

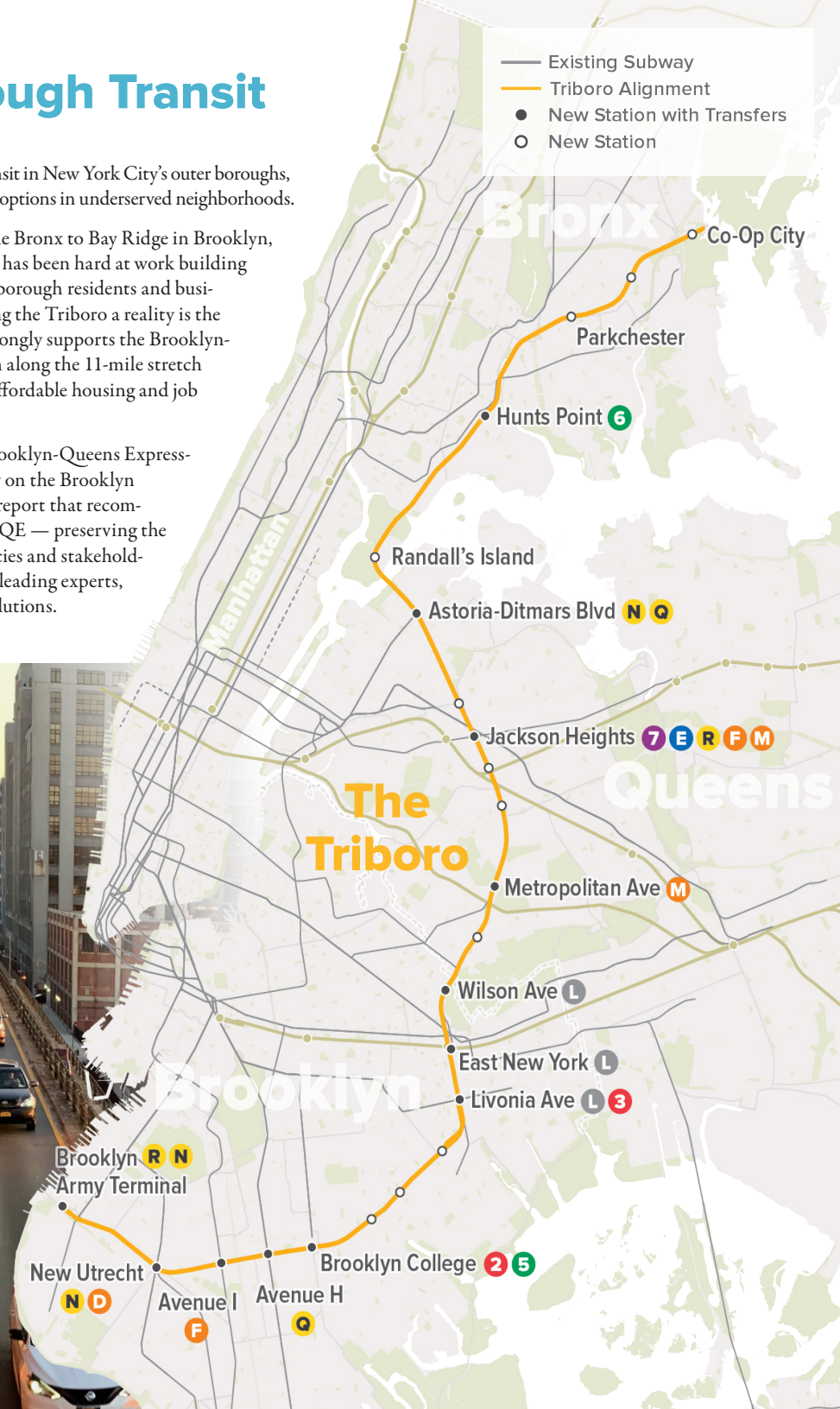
RAIL: Running 24 miles on existing track from Co-op City in the Bronx to Bay Ridge in Brooklyn, the Triboro connects 17 subway lines and 4 commuter lines. RPA has been hard at work building support for the Triboro among key stakeholders, including outer borough residents and businesses, City legislators and the MTA. A crucial first step in making the Triboro a reality is the MTA's commitment to a feasibility study of the line. RPA also strongly supports the Brooklyn-Queens Connector (BQX), a new streetcar system that would run along the 11-mile stretch from Astoria to Red Hook, relieving congestion and promoting affordable housing and job growth along the waterfront.

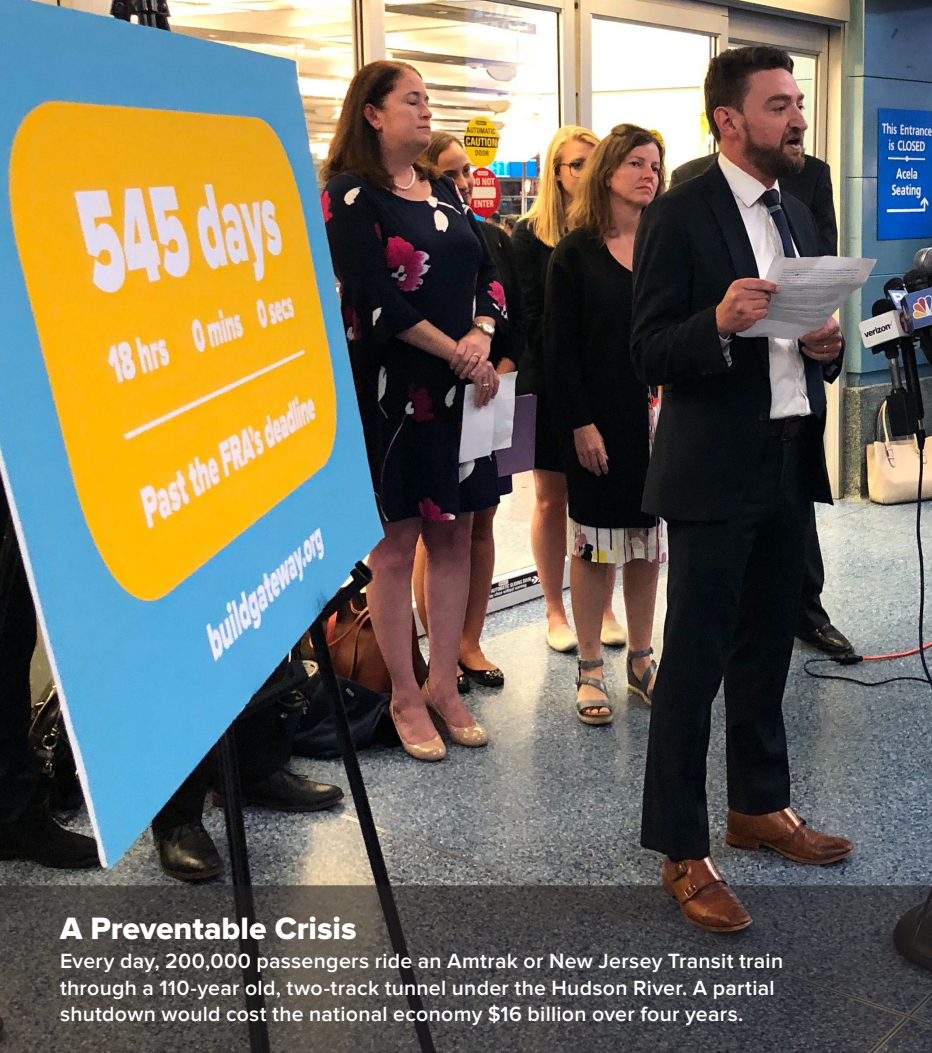
ROADS: When New York City released a plan to rebuild the Brooklyn-Queens Expressway's triple cantilever that called for a temporary six-lane highway on the Brooklyn Promenade, RPA got to work on *Reimagining the BQE*, a pivotal report that recommended demand management strategies to reduce traffic on the BQE — preserving the Promenade and fundamentally shifting the dialogue among agencies and stakeholders. The report spurred the Mayor to appoint a panel of the City's leading experts, including RPA's President Tom Wright, to examine innovative solutions.



Reducing Traffic, Preserving Parks

By enacting policies that significantly reduce traffic, NYC can reduce the number of lanes needed on the BQE and preserve park space.





A Preventable Crisis

Every day, 200,000 passengers ride an Amtrak or New Jersey Transit train through a 110-year old, two-track tunnel under the Hudson River. A partial shutdown would cost the national economy \$16 billion over four years.

Improving Cross-Hudson Rail

The RPA-led Public for Penn coalition has been hard at work building support for a comprehensive vision for the greater Penn Station area among city and state agencies, elected officials, key stakeholders, and the general public. Our upcoming report will measure projected ridership at Penn Station and the need for major improvements to the infrastructure at the station, including the need for better wayfinding, accessibility, safety measures, and amenities. The report also will also consider a plan for moving Madison Square Garden and how to help revitalize the surrounding business district.

Launched by RPA in July 2018, the Build Gateway Now Coalition engages a broad range of stakeholders, including the construction and real estate industries, environmental groups, transit advocates, and other business and civic organizations. This year we published *A Preventable Crisis*, which quantifies the economic impacts of a partial shutdown of the tunnels. The report immediately led to bills introduced in Congress by Senator Chuck Schumer, Rep. Pete King and Rep. Josh Gottheimer, and has been cited dozens of times by Federal and State elected officials over the course of the year. From press conferences to providing testimony, RPA continues to lead the effort to [#BuildGatewayNow](#).

"If we don't take action now to address the nightmare that is Penn Station in a bold and comprehensive way, the rampant delays, emergency incidents, and dangerous overcrowding will only get worse. There is a tremendous window of opportunity now to reimagine Penn and the surrounding area and bring transformational change to the daily lives of hundreds of thousands of commuters."

— **Brian Fritsch**

Manager, Advocacy Campaigns





Promoting Health Equity Nationally



Healthy Regions Planning Exchange

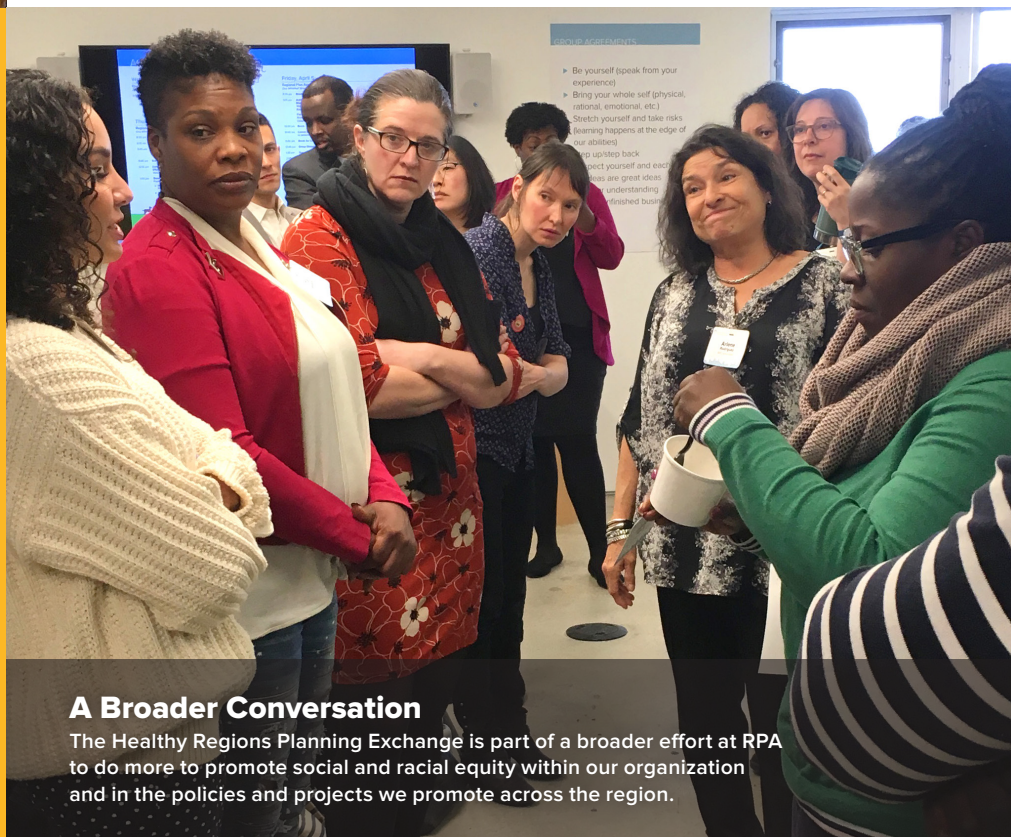
With support from the Robert Wood Johnson Foundation and Bloomberg Philanthropies, RPA has been working with a group of 33 planners, public health officials, and community advocates from 11 regions across the country to exchange methods and ideas to incorporate health equity in regional planning. From Nashville, Tennessee to the Pine Ridge Reservation in South Dakota, long-standing municipal planning associations to grassroots community organizers, Planning Exchange participants represent a diverse group of organizations that approach planning from different perspectives united in a commitment to creating healthier regions.

In April, RPA hosted the group for a three-day symposium centered on how meaningful community partnerships inform better planning, and how race and racism shape our cities and regions, often to the detriment of communities of color. The dialogue has spurred a seven-part webinar series. Topics included *Development without Displacement* featuring Planning Exchange participants Asad Aliweyd and Tia Williams from the Twin Cities region, *Centering Planning Work on Race* with Dr. Mindy Fullilove and Molly Rose Kauffman, and *Transportation Equity* with Tamika Butler from Toole Design in Los Angeles and Ryan Russo, head of Department of Transportation in Oakland, CA. Future topics will include climate justice, representing marginalized communities with data better, and how to get work that centers around race, equity, and health funded.

Our second group convening is set for March 2020, during which we'll begin to outline how to implement the lessons we have learned in our respective regions.

“Our Healthy Regions Planning Exchange is the sort of project that epitomizes why I love working at RPA. By bringing together practitioners from across the country to discuss how to better incorporate health and equity into regional planning, we’re able to address big challenges in our built environment and urban systems in a way that can support communities in regions everywhere.”

— Vanessa Barrios



A Broader Conversation

The Healthy Regions Planning Exchange is part of a broader effort at RPA to do more to promote social and racial equity within our organization and in the policies and projects we promote across the region.



Poised for Progress

Strong neighborhoods, a growing business base, and transportation connections are putting Newark on the spot to manage the equitable growth challenge.



"Newark is an incredibly exciting place to be working — so much is poised to take place there in housing, employment, transit, and aviation."

— **Nat Bottigheimer**, New Jersey Director

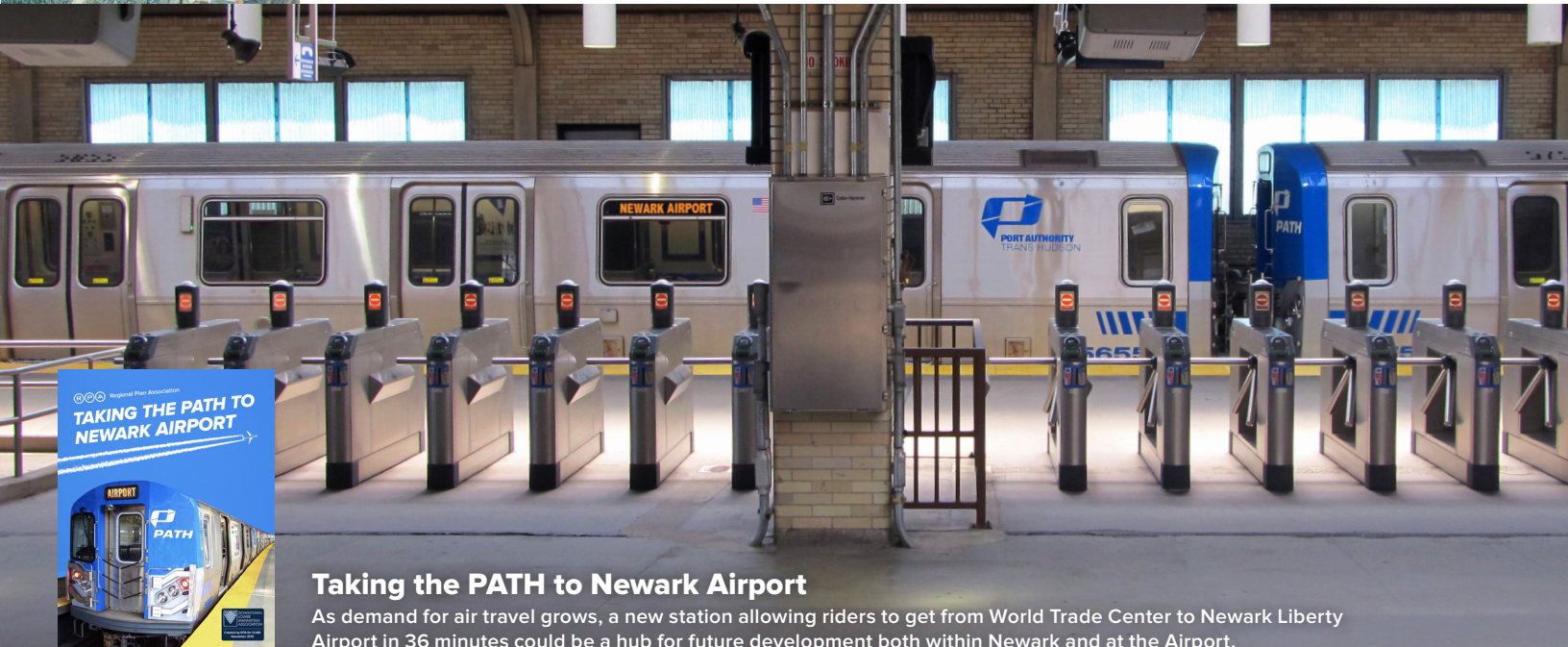
Investing in NJ: Shorelines to City Streets

In New Jersey, RPA's research and advocacy team is focusing on high-impact transportation projects — from extending the PATH rail system to Newark Airport and expanding access for Newark residents, to pressing for increased state funding for transit programs. This year, we also kicked off a vigorous program of urban park development planning, with outreach to underserved communities.

Looking ahead, RPA will continue to engage with the Port Authority of New York and New Jersey (PANYNJ) as they move forward on the many capital projects included in the updated 10-year capital plan. RPA staff will work with the

PANYNJ to ensure that Fourth Plan goals are a key part of any new projects for the region. We will accelerate our planning for growth and trans-Hudson transportation reliability, an effort that will look at all the systems that connect New Jersey and New York.

RPA will also continue to advance New Jersey initiatives in climate change mitigation and adaptation, park and open space development, incubating business for a burgeoning offshore wind industry, and anchor institution investment in communities.



Taking the PATH to Newark Airport

As demand for air travel grows, a new station allowing riders to get from World Trade Center to Newark Liberty Airport in 36 minutes could be a hub for future development both within Newark and at the Airport.



Localizing Climate Adaptation Solutions

The impetus for our Equitable Adaptation project stems from the fact that communities of East Elmhurst and Corona as well as other low-income communities of color experience climate change differently than other communities such as the coastal towns on Long Island, Connecticut, and New Jersey. Instead of dealing with rising sea levels which cause storm-related flooding, inland communities experience extreme weather events which can create a domino effect that affects a resident's ability to get to their job or school, their physical and mental health, and their safety.

RPA and Make the Road New York embarked on a partnership in 2018, kicking off a series of engagement and listening sessions to better understand the challenges experienced

by their members and community organizers. The result of this project was a publication of adaptation strategies directly informed by Central Queens residents. This collaborative effort has continued to advance priority strategies identified in community workshops. The three focus areas are empowering individuals with tools and information, improving and expanding impact of community trainings, and positioning policy.

RPA and Make the Road New York are producing a healthy homes guide geared toward tenants, recommendations for community health worker trainings, and a climate action manual meant to be used by both organizers, critical stakeholders, and elected officials to push for policies that can better equip Queens communities to mitigate, adapt, and thrive.

Growing Connecticut's Urban Centers

In Connecticut, RPA has been actively engaged with the Lamont administration and stakeholders in Fairfield, New Haven, and Hartford counties to advance key transportation and affordable housing priorities. Building on the momentum of our Connecticut Director's co-chairmanship of the Governor's Transition Transportation Policy Committee, RPA is advocating for tolling legislation to raise revenue for transit improvements. We have also been working with a coalition of advocates and the State Department of Housing to advance best practices in planning and zoning for affordable housing and equitable transit-oriented development.

Through our 2019 Sustainable Cities Partnership Program, RPA is working on the ground with community partners to realize the Fourth Regional Plan Flagship Place vision for Bridgeport as the center of a new green economy in Connecticut. Initiatives that we're supporting through this effort include advancing equitable economic growth in partnership with the City and its higher education, as well as healthcare institutions and working with youth organizers and Greater Bridgeport Transit to improve access to bus transit and safe routes to school. In addition to this partnership with Bridgeport, we are engaging with Connecticut's largest urban centers — Stamford, Bridgeport, New Haven, Waterbury, and Hartford — to create and communicate a Cities Agenda to influence public policy and address the challenges and opportunities for economic growth in the state.



Bridgeport Moving Forward

Sound urban policies are supporting equitable economic growth, attracting people and jobs and ensuring that longtime residents benefit from new investment in Bridgeport.



"We are creating a dialogue among municipal leaders, residents, and other community stakeholders throughout Fairfield County to address residential segregation and apply tools on the ground to change the systems that perpetuate inequality."

— **Melissa Kaplan-Macey**

Vice President, State Programs & Connecticut Director



New City Parks

RPA is pleased to welcome former New York State Parks Commissioner Rose Harvey who is leading this initiative.

Turning Parking Lots into Parks

The purpose of the New City Parks (NCP) initiative is to create new or revitalized urban park systems in and for neighborhoods of need. In the first year, NCP will assess more than 20 cities in the Northeast or Rust-Belt and select up to six where the need, impact, and opportunity to act now is greatest. NCP will then complete the business plans and necessary due diligence to set up and make ready for construction “on the ground pilots” as prototypes and to create momentum for build out, use, and programming of entire systems.

NCP’s feasibility analysis will marry need with viability, looking for cities with mayoral support and agencies open to action; with available space; with the potential for upfront public and private funding; where there is an ability to forge multi-sector partnerships; and with potential stewards and partners who will program, maintain, and ensure the long-term sustainability of these systems.

NCP and its respective partners will have a vision for an entire system, identified future funding, engaged community, and created well-used pilot parks. Measures of success will be the number of safe and sustainable pilot parks completed, the number of people served, the depth of programming for local children, and the degree of local buy-in.

"Parks are places to play, to commune, to escape into nature, to build mind and muscle. They are part of our DNA and should be part of every urban neighborhood."

— Rose Harvey
Senior Fellow,
Parks & Open Space





RPA in the Region

Thank you to everyone that joined us over the course of the year to discuss, debate, and get the word out about all the critical work we do at RPA. From our biggest day of the year, our 1,000+ person spring Assembly featuring JP Morgan Chase CEO Jamie Dimon, Los Angeles Mayor Eric Garcetti, and Connecticut Governor Ned Lamont, to our Hudson River Tunnel briefing with Senator Chuck Schumer, to our multiple issue-based roundtable discussion and infrastructure tours, we appreciate your support.



A Preventable Crisis

The Economic and Human Costs
of a Hudson River Rail Tunnel Shutdown



Thank You!

Thank you to all of RPA's members and funders for supporting our work. Your partnership makes the progress highlighted in this annual report and other initiatives possible.

To become an RPA member or to make a donation, please visit rpa.org/donate. We would love to have you be part of the RPA family!



Contributions

\$500,000 and above

Robert Wood Johnson Foundation

\$100,000 – \$499,999

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Upper Manhattan
Empowerment Zone
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& Mittendorf
WXY Studio

\$1,000 – \$4,999

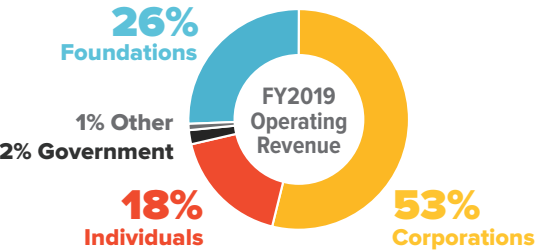
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Government

The City of New Haven
Suffolk County, New York
NJ Transit
Norwalk Redevelopment
Agency
Metropolitan Trans-
portation Authority

Financials



Operating Revenue	FY2019
Foundations	\$1,312,700
Corporations	\$2,751,427
Individuals	\$894,972
Government	\$105,753
Other	\$46,727
Total Operating Revenue	\$5,111,579

Endowment Revenue	
Peter Herman Chair	\$20,000
Dividends and Capital Gains	\$129,682
Total Endowment Revenue	\$149,682

Total Revenue	\$5,261,261
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Operating Expenses	
Research	\$3,061,151
Public Affairs	\$447,009
Management and General	\$667,688
Fundraising	\$881,883
Total Operating Expenses	\$5,057,731

Net Surplus	\$203,530
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Total Assets	\$5,389,732
Total Liabilities	\$435,498
Total Net Assets	\$4,954,234



Regional Plan Association

Regional Plan Association is an independent, not-for-profit civic organization that develops and promotes ideas to improve the economic health, environmental resiliency and quality of life of the New York metropolitan area. We conduct research on transportation, land use, housing, good governance and the environment. We advise cities, communities and public agencies. And we advocate for change that will contribute to the prosperity of all residents of the region. Since the 1920s, RPA has produced four landmark plans for the region, the most recent was released in November 2017. For more information, please visit rpa.org or fourthplan.org.

New York

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212.253.2727

New Jersey

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and 60 Union Street, Suite 1-N, Newark, NJ 07105
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Samuel I. Schwartz
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Tokumbo Shobowale
Anthony Shorris

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Ryan Simonetti
Jennifer Skyler
Monica Slater Stokes
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Kate Wittels
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