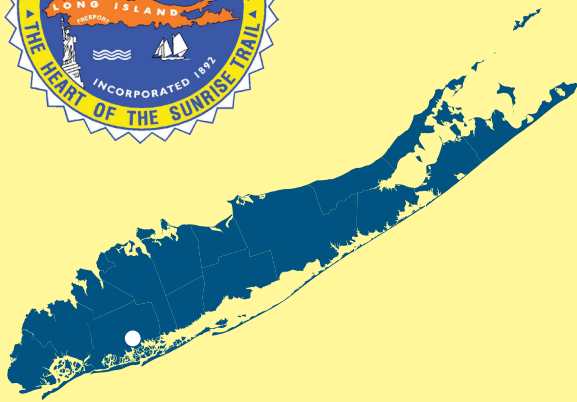
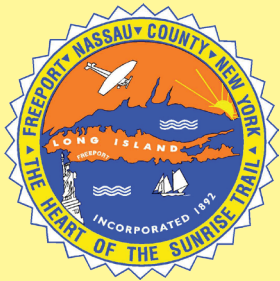


Building a Better Freeport

Proposed Overlay District Zoning Regulations and Design Guidelines Village of Freeport, NY



July 2010

Prepared for:

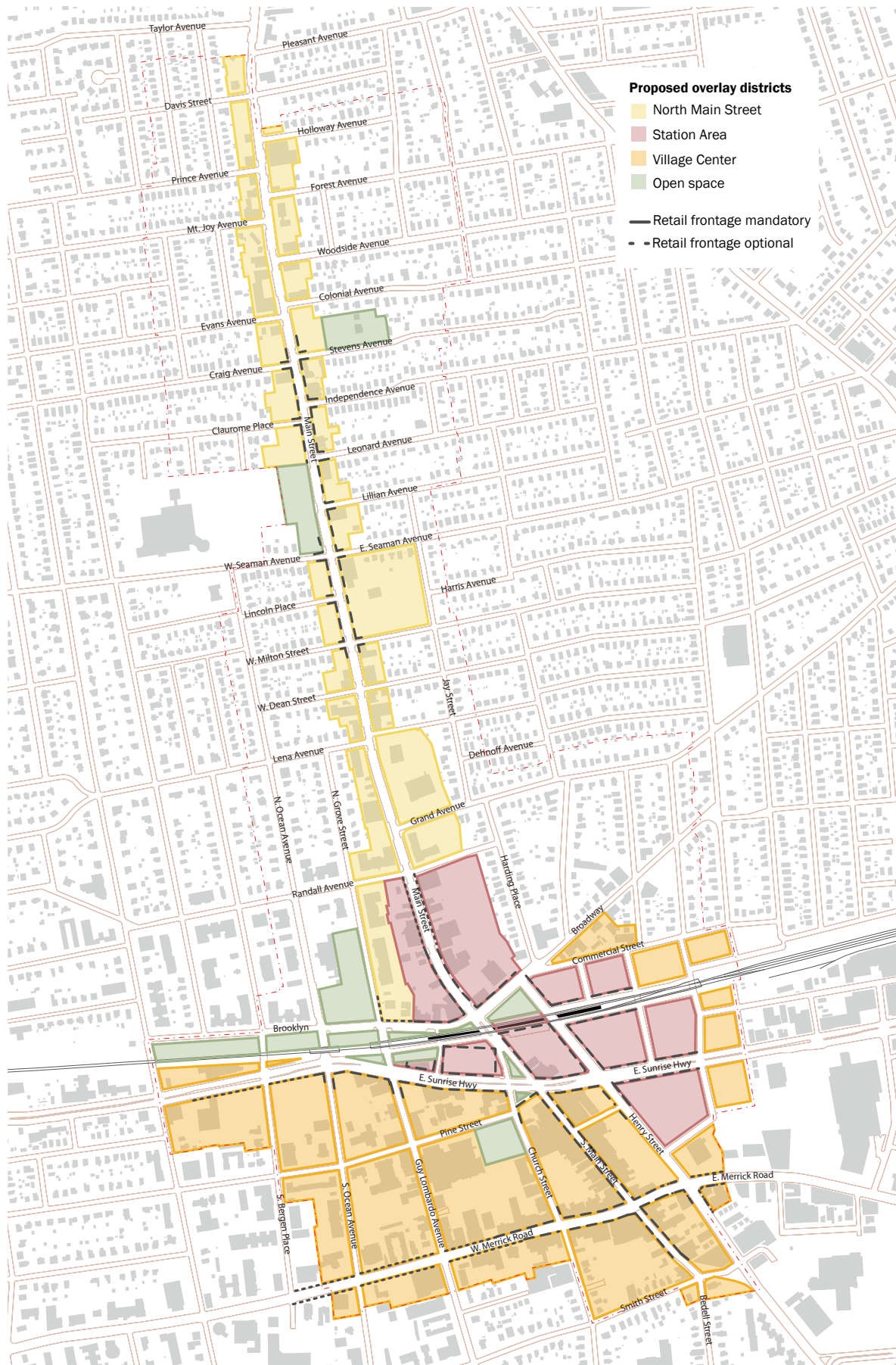
Mayor Andrew Hardwick
Carmen J. Pinyero – Deputy Mayor, Village Trustee
Robert Kennedy – Village Trustee
Jorge A. Martinez – Village Trustee
William H. White, Jr. – Village Trustee
The Freeport Community Development Agency



Existing zoning



Proposed overlay districts



Introduction

The purpose of the following Overlay District Zoning Regulations and Design Guidelines is to coordinate and orchestrate the revitalization and growth of downtown Freeport in a way that furthers the goals articulated in the Freeport Master Plan. With sound planning, sensible zoning and smart design guidelines, Downtown Freeport can grow into the livable, walkable, safe and economically strong community that residents know it can become.

For well over a half-century, municipalities have used the tool of zoning to shape their future growth. While zoning can effectively regulate how buildings are used, it says little, if anything, about how buildings and the spaces between buildings should look and feel – a factor that is at least equally important as use in how successful and desirable a community is. Conventional zoning often also does not take into consideration sites' particular locations and access to certain amenities such as transit. Take the issue of parking for example. Many zoning codes, including Freeport's, dictate how much parking is required according to how a building is used, without consideration of a location that might be downtown or near the LIRR station, where large parking facilities are not necessary or desirable. Zoning codes also generally do not specify where and how parking ought to be located on the site. Sensitive urbanists will know that parking that is located in front of buildings and require curbcuts deter from a vibrant pedestrian environment – parking that is located behind buildings and is accessed through side streets, by contrast, enhance the attractiveness and safety of the street. The following Overlay District Zoning Regulations and Design Guidelines identify a set of concrete means for Freeport officials to ensure that new development contributes positively to the community and its urban strengths.

Three Overlay Districts

The Public Participation process identified three distinct districts in the study area. First, the Station Area – several large blocks directly adjacent to the train station, that are now used for a variety of purposes, some more intensely than others. Second, the Village Center, south of Sunrise Highway, characterized by small, community-oriented retail and other uses. And finally, North Main Street, what is currently a car-oriented strip of underperforming buildings but could become the attractive gateway to Downtown Freeport from the north, with viable local businesses and homes lining both sides of the street.

Development in each district will be subject to the corresponding District Overlay Zoning Regulations described in this document. Overlay District Zoning Regulations include new regulations in areas currently covered by Freeport's existing zoning (such as allowed uses, building bulk and setback) and override them. They also include specifications in areas not currently addressed by Freeport's existing zoning, such as the location of parking and the design of buildings. Specific quantifiable design standards are included when possible, but the Village of Freeport will need to exercise some discretion when reviewing proposed projects.

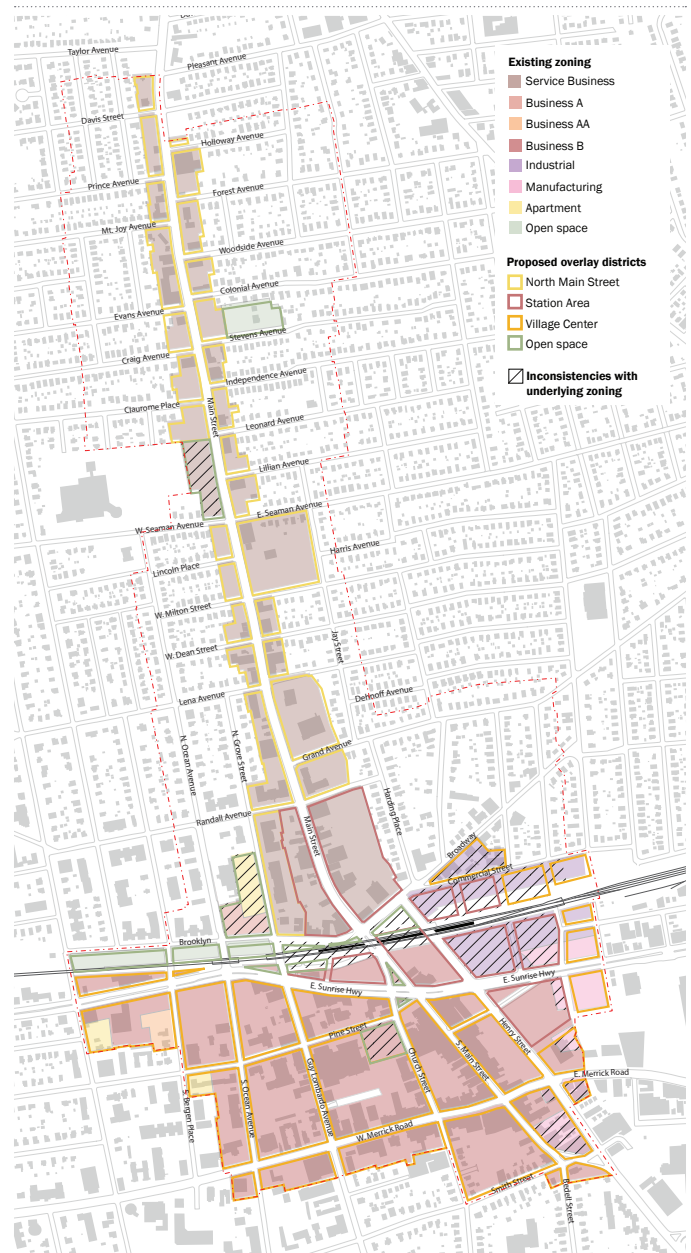
Specifying these regulations up front helps potential developers to know the Village's expectations regarding new development. The Village can also provide an expedited permitting process to developers who follow these guidelines and regulations.

Density where it belongs – around transit: The Station Area Overlay District

This district, part of the TOD area identified in the Master Plan, is currently characterized by a hodgepodge of different uses and aesthetics. No wonder, since the area is split up between four different zoning designations: from “Service Business” on North Main Street and “Business B” south of the station, to “Manufacturing” and “Industrial” to the east.

A single Station Area Overlay District will create a uniform district that builds on its unparalleled access to transit, and the benefits that such access confers: density and activity without the burden of needing to accommodate large amounts of parking. The Station Area Overlay District includes two- to eight-story buildings that still feel of a pedestrian scale. Uses are mixed, but

Overlay District Zoning Regulations overrides existing zoning. Those few sites, however, whose underlying zoning is fundamentally at odds with the proposed Overlay District Zoning Regulations, should be rezoned to a more compatible designation.



retail frontage is encouraged and even required in key locations. Streetscapes are designed to facilitate access to and from the train station. Parking requirements are reduced and a shared parking scheme is implemented.

The new zoning will phase out uses such as automobile service shops and large manufacturing warehouses, while phasing in such uses and lounges and pubs, parking structures, cinemas and performing arts theaters, artisan and craft production, indoor recreation, libraries and museum and multi-family residential buildings, among other uses.

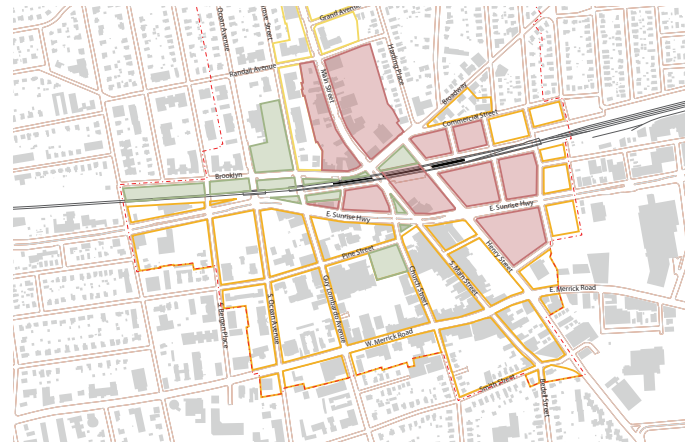
The Station Area presents the greatest opportunity for redevelopment in the Village of Freeport. Whether through infill development on unbuilt and vacant spaces between buildings or through wholesale redevelopment of existing sites like parking fields and warehouses, the goal for the Station Area is to transform it into a regional, mixed-use center with residential, commercial and retail uses and open space, all within walking distance of the LIRR station.

The Station Area lies at the heart of the Village and as improvements here are realized, it will serve to pump the dividends of its success northward along North Main Street as well as southward towards the existing Village center around South Main Street. Revitalization here will serve to bridge what has traditionally been a divide between the northern and southern sections of Freeport and help to facilitate a connected, highly functioning community.

Purpose & overall considerations

The purpose of the Station Area Overlay District is to promote a compact mixed-use district organized around the LIRR train station and station-oriented retail, residential and office space.

- Two- to eight-story buildings with varied massing for larger projects to ensure a town-scale architecture.
- Buildings are at the sidewalk line (no front set-back) in order to clearly define the public realm of streets and public spaces. At least 75% of any frontage shall align with established “build-to” lines.



- Buildings are pedestrian/sidewalk friendly. Public entrances are off of the street. Service functions, including parking, loading, trash and utilities are screened from pedestrian view, behind the building whenever possible.
- Streetscapes are designed in support of connecting to the station with the surrounding community.
- Parking ratios, especially for any new residential uses, are reduced as much as possible in response to the new access to transit and the potential for shared parking, both among uses and with commuter parking.
- Parking is managed in a way that does not compromise the pedestrian orientation of this place. Structured parking as well as shared parking schemes are encouraged. Surface parking lots must be broken up into small increments and placed behind buildings so that they do not interfere with the continuity of the street wall. No more than 25% of any frontage can be screened parking.
- Streetscapes and civic spaces are urban and planted.

What is currently a parking lot surrounded by little activity will be transformed into a Village gathering place surrounded by mixed-use residential and retail buildings.

Before



After



The area around the LIRR station will become a safe, enjoyable center for nightlife and culture for Freeport residents and visitors alike

Before



After



Building on the best of Freeport: The Village Center Overlay District

This district, identified as the South Main Street area in the Master Plan, epitomizes the best qualities of today's Downtown Freeport: a good pedestrian environment, local businesses, mixed-use buildings of a suburban downtown scale. The Village Center Overlay District will emphasize these qualities and ensure that new buildings achieve a high level of vibrancy, while old buildings that do not are phased out. The changes to this area are less dramatic than the North Main Street Corridor and Station Area, but are just as important to direct and shape investment and enhance the sense of "place."

The Village Center Overlay District help provide a transition from the Station Area to residential neighborhoods around downtown, allowing for buildings up to four stories in height, with a similar mix of uses as around the train station. Parking regulations encourage visitors to park once and visit several destinations downtown. Retail frontage is allowed and encouraged.

Purpose & overall considerations

The purpose of the Village Center Overlay District is to promote a context-sensitive mixed-use district in downtown Freeport.

- One- to four-story buildings with animated changes in massing.
- Buildings are at the sidewalk line (no front set-back) in order to clearly define the public realm of streets and public spaces. Typically, 75% of any frontage shall align with established "build-to" lines.
- Buildings are pedestrian/sidewalk friendly. Public entrances are off of the street. Service functions, including parking, loading, trash and utilities are screened from pedestrian view, behind the building whenever possible.
- Streetscapes are designed in support of encouraging residents, workers and shoppers to park once and walk around in downtown Freeport.
- Parking ratios, especially for any new residential uses, are reduced due to access to transit and the potential for shared parking among uses.
- Parking is managed in a way that does not compromise the pedestrian orientation of this place. Structured parking as well as shared parking schemes are encouraged. Surface parking lots must be broken up into small increments and placed behind buildings so that they do not interfere with the continuity of the street wall. No more than 25% of any frontage can be screened parking.
- Streetscapes and civic spaces are urban and planted.



The View across the Triangular Median towards the South Main Street District with new infill and restored buildings





A new identity for Freeport: The North Main Street Overlay District

This district is among Freeport's less successful urban environments. Traffic is dangerous and a nuisance. Pedestrian amenities are limited. Buildings are hidden behind parking. The North Main Street Overlay District reimagines a new identity for the corridor as the new entry into downtown Freeport and the LIRR station from the north. North Main Street Overlay District Zoning Regulations and Design Guidelines encourage a more attractive, uniform and compact look, at a suburban downtown scale. New residential and commercial buildings all face North Main Street with stoops, porches, shopfronts and awnings at the ground level to activate the street with pedestrian activity. Building heights will vary up to three stories, with all entries directly onto the street and parking behind the buildings.

North Main Street is too long to support continuous, intensive commercial uses along the entire corridor, but it is well primed for an intensive mixed-use node between Stevens and Milton Streets, with residential mixed-use to the north and south. Automobile service shops and other uses that detract from the pedestrian potential of North Main Street will be phased out, though existing businesses will be grandfathered in.

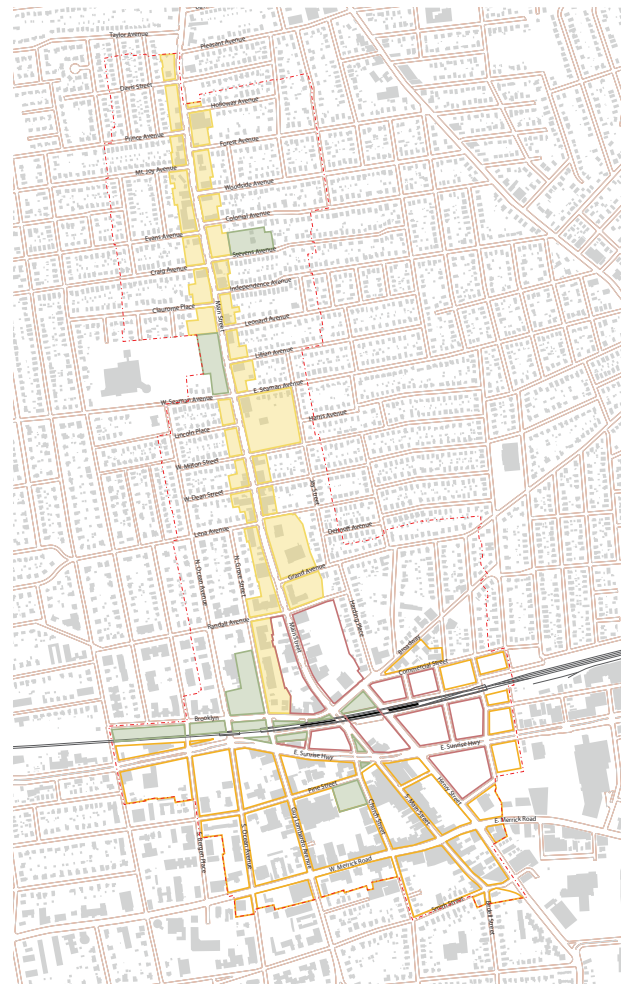
The bus depot between Leonard Street and Seaman Avenue will be converted into a park and will become a gravity point for the district.

Purpose & overall considerations

The purpose of the North Main Street Overlay District is to create an attractive entry to Downtown Freeport and the LIRR train station with consistent and pedestrian-friendly building forms.

- Buildings can be as tall as three stories. Their height and bulk will be uniform so as to encourage a more attractive, uniform and compact look at a suburban downtown scale.
- All new buildings face North Main Street, and shopfronts, stoops and porches at the ground level activate the street for pedestrian activity.
- Retail activity is concentrated between Stevens and Milton Streets, around a new park where the bus depot currently stands.
- Streetscapes are designed to accommodate a variety of users, including pedestrians, bicyclists, bus users and drivers. Special attention is paid to the connection to the train station.

- Parking is located behind buildings and accessed from side streets, so as to not compromise the pedestrian orientation of the street.



Proposed overlay district zoning regulations

Scope

The following regulations shall apply to the Station Area, Village Center and North Main Street Overlay Districts, except when specified.

Permitted & prohibited uses

All other uses not herein specifically permitted (marked with the ✓ symbol in the table below) are prohibited without a special use permit and site plan review.

One or more land uses identified as being allowable in the district may be established on any parcel within that district.

	Station Area	Village Center	North Main St
Retail			
Bar, tavern and night club	✓	✓	✓
General retail, except w/ any of the following features	✓	✓	✓
Alcoholic beverage sales	✓	✓	✓
Auto or motor vehicle service / gas stations	✓	✓	✓
Motor vehicle sales (indoor showroom)			
Motor vehicle sales (out-door lot)			
Drive-through facilities			
Floor area over 20,000 (per business)			✓
On-site production of items sold	✓	✓	✓
Tattoo parlors	see Chap. 210-71		
Shooting galleries, penny or picture arcades	see Chap. 210-71		
Neighborhood market/convenience store	✓	✓	✓
Eating establishments, except drive-through	✓	✓	✓
Outdoor vending	✓	✓	✓
Small nurseries, garden shops	✓	✓	✓
Large nurseries			✓
Service General			
Banquet facility / catering	✓	✓	✓
Child day care, more than 8 and up to 14 children	✓	✓	✓
Day care center, child or adult	✓	✓	✓
Drive-through service			

Lodging - bed & breakfast or inn	✓	✓	✓
Lodging - hotel	✓	✓	✓
Mortuary, funeral home	✓	✓	✓
Vehicle repair (passenger and commercial vehicles)			
Services: Business, Financial, Professional			
Office - general purpose	✓	✓	✓
Bank, financial services	✓	✓	✓
Medical services: clinic, urgent care		✓	✓
Medical services: doctor, dentist, chiropractor, etc, office	✓	✓	✓
Medical services: extended care			✓
Transportation, Communications, Infrastructure			
Parking facility (garage) - public or commercial			
Parking facility (open air)			
Transit station or terminal	✓		
Public transit shelter	✓	✓	✓
Industry, Manufacturing, Processing, Warehousing, Distribution			
Artisan/craft product manufacturing	✓	✓	✓
Furniture and fixture manufacturing, cabinet shop	✓	✓	✓
Laboratory: medical, analytical	✓	✓	✓
Media production, office or storefront type (no sound stage)	✓	✓	✓
Data processing			✓
Printing and publishing			✓
Warehousing and distribution			
Outdoor storage			
Heavy manufacturing (e.g. blacksmith, coal, concrete, cement, etc.)			
Recreation, Education, and Public Assembly			
Adult business	see Chap. 210-26		
Lodges and clubs	✓	✓	✓
Religious assembly	✓	✓	✓
Health/fitness facility	✓	✓	✓

Indoor recreation facility: commercial	✓	✓	✓
Library, museum	✓	✓	✓
School - public or private	✓	✓	✓
Theater - cinema	✓	✓	✓
Theater - performing arts	✓	✓	✓

Residential Uses

Emergency/transitional shelter			✓
Home occupation	✓	✓	✓
Live-wWork use/joint living-working quarters	✓	✓	✓
Ground floor residential	✓	✓	✓
Residential care facility for the Elderly	✓	✓	✓
Residential care facility for 7 or more clients	✓	✓	✓
Caretaker residential use	✓	✓	✓
Carriage house/second dwelling, single dwelling		✓	✓
Multi-family building	✓	✓	✓

Operational standards

All business activities shall be conducted and located within an enclosed building, except that the following business activities, to the extent permitted, may be conducted outside of an enclosed building:

- News stands
- Flower stands
- Recreational or entertainment uses
- Art displays

Any activity should not emit noise levels above 65 dB as measured at the property line.

Outdoor storage of materials, products, equipment or vehicles shall be screened by a solid wall from the street. Materials, products or equipment shall not be piled higher than the height of the fence or wall, nor encroach into required parking and landscape areas.

All property shall be maintained in a safe, sanitary and attractive condition, including but not limited to, structures, landscaping, parking area, walkways, and trash enclosures.

Parking requirements

- Parking requirements shall be as follows in Station Area, Village Center and North Main Street Overlay Districts.

	Station Area	Village Center	North Main St
Residential (studio & 1 BR)	1/unit	1/unit	1/unit
Residential (2+ BR)	1.25/unit + 0.1/unit (guest)	1.25/unit + 0.25/unit (guest)	1.5/unit + 0.25/unit (guest)
Live-work	2/unit	2/unit	2/unit
Non-residential	2/1,000 SF	2/1,000 SF	2.5/1,000 SF

- Parking requirements can be fulfilled by:
 - › Paying an in-lieu fee to the municipality;
 - › Sharing parking with adjacent uses that have different peak periods during the day (follow the methodology developed by the Institute of Transportation Engineers and Urban Land Institute in "Shared Parking, 2nd Edition," 2005);
 - › Allowing required spaces to be located in a parking structure or parking field off-site, up to 500' from the development site;
 - › Allowing up to 20% of parking requirement to be satisfied with on-street parking adjacent to the development site if approved by the Zoning Board;
 - › Providing parking on-site.
- A 10% to 50% parking requirement reduction can be granted by the Zoning Board for affordable housing, or housing for students, seniors or people with disabilities.
- All parking structures and parking lots for commercial, recreational and institutional uses shall include sufficient bike racks to allow the parking of a minimum of one bike for every 12 automobiles or one bike for every 3,000 square feet of building floor area, whichever is greater. Half of those bicycle parking spaces are to be covered.

Parking location & design

- Any off-street parking is located behind the buildings and never between the sidewalk and the building frontage. Vehicular access to the parking is provided from a side street, as close as possible to the rear of the lot. If this is not possible, parking is located to the side of the building. When parking is located in a side yard and has frontage on a public right-of-way, no more than 25% of the total site's frontage or 40 feet, whichever is less, shall be occupied by parking lot.
- Where parking areas have to be along the edge of a sidewalk or other public space, the parking lots must be properly screened with landscaping or with an architectural wall or trellis of 3' to 5'. Tall chain link fences are not acceptable.
- When possible, structured parking should be internal to the block and wrapped with a liner building that includes residential, office or retail space, and hides the parking facility from the street. If a liner building is not possible, a parking structure

should, at minimum, include usable building space to a depth of at least 20 feet for a use such as retail.

- All parking structures are to have an architecturally finished façade that is compatible in scale, texture, and overall design with nearby buildings.
- No one expanse of parking shall be larger than 40 spaces. Parking areas shall be designed and landscaped to avoid long, uninterrupted rows of vehicles by breaking them into separate parking lots divided by tree lines, alleys, pedestrian areas, or buildings.
- Parking lots should be made of a permeable surface when possible.
- Pedestrian circulation elements such as sidewalks, crosswalks, landscaping and lighting shall be incorporated into the design of parking lots.
- Parking facilities can be shared by two or more uses.
- The cost of parking shall be "unbundled" from the cost of rent. Tenants shall be charged a separate fee for the privilege of a parking space, in addition to the cost of rent.
- Except for schools, child care centers, hotels and passenger terminals, there shall be no drop-off or valet parking services that interrupt the street sidewalk.

Vehicular access; curb cuts

- Curb cuts on the main streets should be avoided whenever possible. Any necessary curb cuts should be preferably located on side streets.
- Adjacent properties share curb cuts whenever possible.
- Curb cuts are not to be placed within 20 feet of street corners, or within 3 feet of utility poles or hydrants. There are no restrictions on how close to the property line a curb cut can be located, particularly if the curb cut is shared by adjacent developments.
- Curb cuts are limited to 12' wide for one-way and 16' wide for two-way.
- Loading and unloading areas shall be located in the back of buildings. If that is not possible, the side of building is acceptable. Vehicular access to these loading areas shall be provided from side streets. Whenever possible, loading zones and access to loading docks shall be shared by two or more adjacent buildings.

Restrictions on planting

- Street tree spacing is to average 30 feet on center, and may be planted in continuous trenches where appropriate.
- Trees may be planted as close as 5 feet from the curb of an intersection or driveway, as prescribed in the Master Plan's landscaping plan, provided sight distances remain safe.
- Hedges or shrubs may be planted between the sidewalk and the curb in appropriate places where access of the curb by cars has been adequately accommodated.

Building height, setbacks & massing

Building heights, setbacks and massing regulations vary by Overlay District. District-specific regulations are described below.

Station Area

All buildings and structures in the Station Area Overlay District shall have a minimum of 2 stories and a maximum of 8 stories.

# stories	Max. ratio of each story (% of lot area)							
	1	2	3	4	5	6	7	8
2	100	100						
3	100	100	100					
4	100	100	100	85				
5	100	100	100	85	85			
6	100	100	100	100	85	85		
7	100	100	100	100	85	85	85	
8	100	100	100	100	85	85	85	85

Village Center

All buildings and structures in the Village Center Overlay District shall have a maximum of four stories.

# stories	Max. ratio of each story (% of lot area)			
	1	2	3	4
1	100			
2	100	100		
3	100	100	100	
4	100	100	100	85

North Main Street

All buildings and structures in the North Main Street Overlay District shall have a maximum of three stories.

# stories	Max. ratio of each story (% of lot area)		
	1	2	3
1	100		
2	100	100	
3	100	100	100

Individual story requirements

	Height of story, by use	
	Residential and non-residential	Parking
Upper floors	10' min.	8' min.
Ground floor	14' min.	8' min.
Basement floor	9' min.	8' min.

Lot coverage

The entire lot may be covered by buildings, except within the mandatory open spaces specified in the "Required yards" section.

Required yards

Minimum setbacks required and, where noted, maximum setbacks allowed. No part of any building shall be set back farther than the prescribed maximum setback except where a frontage type standard allows exceptions or establishes different requirements. All setbacks are to be landscaped.

	Min.	Max.
Front yard	0 ft	10 ft
Side yard (to sidestreet)	0 ft	10 ft
Side yard (to neighboring property)	0 ft	5 ft
Rear yard (to neighboring property)	10 ft	No max.
Rear yard (to alley)	3 ft	No max.

Building design

- Buildings are oriented toward the street and/or other public spaces. Buildings' primary entrances should be clearly identifiable, scaled to the size of the street they front, and oriented toward the street. Rear doors, loading docks and service entries are not to be located along street frontages. Buildings are not to have large blank or monotonous surfaces.
- Building facades shall be broken into segments of between 25 feet and 40 feet in length. These segments can be articulated by using different combinations of the following: building materials, small setbacks in the wall plane, and variations in building height, arrangement of windows and roof forms. The goal is to make large buildings appear as a series of side-by-side buildings or bays.
- Blank walls longer than 10 feet shall not face a public street.
- In commercial and mixed-use buildings, the first floor is primarily transparent (at least 65% of the façade area below the height of 10 feet facing a major street is devoted to display windows and/or windows affording some view into the interior areas). Multi-family residential buildings with no retail or office space are exempt from this requirement.
- Buildings situated at corners should "wrap" the corner by continuing façade elements such as the cornice or other horizontal features on all street elevations. They should also possess a level of architectural design that accentuate its prominent location. This can be accomplished through height projections incorporated into a design feature, such as a building peak, tower, or similar accent with the highest point located at the intersecting corner, which may be up to an additional 10 feet above the height limit.
- All architectural openings, including windows, doorways, arches and porch framing, should be constructed with their height equal to or greater than their width, and framed by appropriately scaled lintel or arch at the top and sill at the bottom.

- With the exception of the necessary driveways, walks and entrance areas, building front yards shall be fully graded, landscaped and continuously maintained in a neat and orderly fashion, and no portion of the said front yard shall be used for parking automobiles or other vehicles or storing articles of any kind.

Building frontages

The private frontage is the area between the building and the lot lines. The character and arrangement of private frontages create a particular transitional relationship between the private and public realm. Frontage types are required for all buildings within each overlay district. Any frontage type described below is allowed in all three Overlay Districts, except Porch, which is not allowed in Station Area or Village Center districts. Along key frontages in all districts – as indicated in the map on page 3 – a Shopfront frontage is required for all new construction; a retail use within the building is not required.

The following frontages are described in detail in the pages that follow:

- **Shopfront:** a frontage wherein the facade is aligned close to the frontage line with the building entrance at sidewalk grade. This type is conventional for retail use. It has a substantial glazing on the sidewalk level and an awning that may overlap the sidewalk to the maximum extent possible
- **Forecourt:** a frontage wherein a portion of the facade is close to the frontage line and the central portion is set back. The forecourt created is suitable for vehicular drop-offs. This type should be allocated in conjunction with other frontage types. Large trees within the forecourts may overhang the sidewalks.
- **Stoop:** a frontage wherein the facade is aligned close to the frontage line with the first story elevated from the sidewalk sufficiently to secure privacy for the windows. The entrance is usually an exterior stair and landing. This type is recommended for ground-floor residential use.
- **Porch:** a frontage wherein the facade is set back from the frontage line with an attached porch permitted to encroach. A fence at the frontage line can maintain the demarcation of the yard.

Shopfront

The shopfront frontage is allowed in Station Area, Village Center and North Main Street Overlay Districts.

Shopfronts are large glazed openings in a façade, filled with doors and transparent glass in a storefront assembly. At least 65% of the façade area below the height of 10 feet shall be transparent glass, which should extend to within no less than 3 feet of the ground. This traditional retail frontage type is often provided with canopies or awnings, which may be fixed or retractable, to add a comfortable sense of enclosure and weather protection. The storefront shall provide clear views of merchandise displays within the shop space and/or maintained and lighted merchandise display(s).

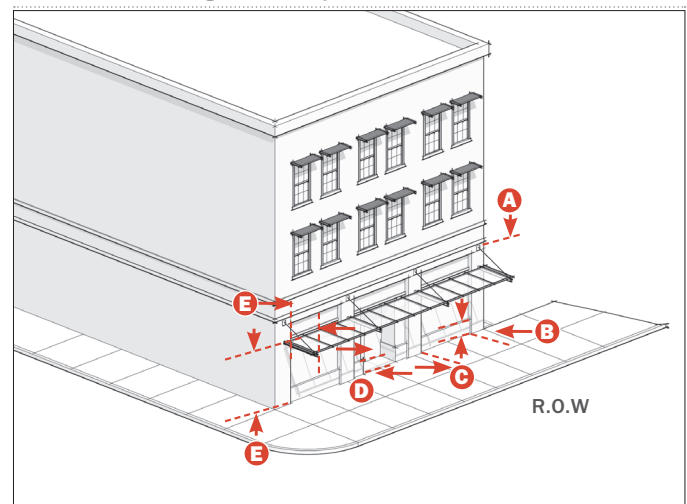
A great variety of shopfront designs are possible, per the following (see axonometric and section diagrams):

- A** min. 10 feet tall, as measured from the adjacent sidewalk.
- B** The corresponding storefront(s) opening(s) along the primary frontage shall comprise at least 65% of the 1st floor wall area facing the street and not have opaque or reflective glazing.
- C** Glazing shall not begin any higher than 3 ft above the sidewalk.
- D** The entry shall not be set back more than 3 ft from the building line.
- E** Awnings, signs, etc., shall be located at least 8 feet above the adjacent sidewalk and may project for the width of the sidewalk at a rate of 6 inches per each foot above 8 feet to a maximum encroachment of within 2 feet of the curb.
- F** Signage shall not project within 2 ft of the adjacent curb face(s).
- G** Awnings shall only cover storefronts and openings so as to not cover the entire facade.

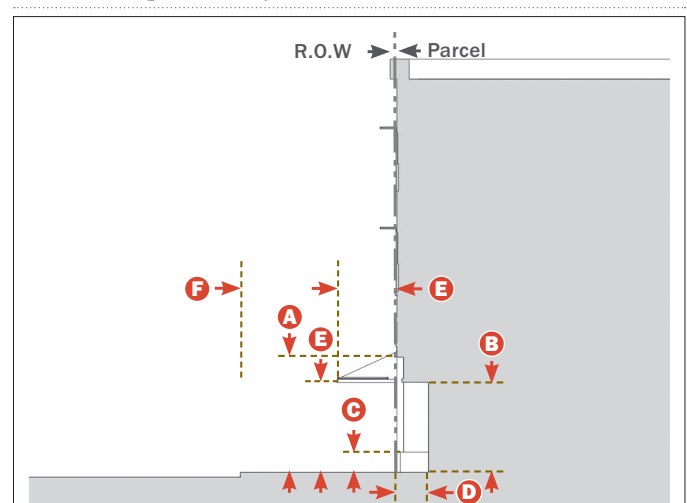
The following signage types are appropriate on shopfront frontages (see Signage section for more information).

Sign type	Allowed?
Marquee	Y
Awning	Y
Projecting	Y
Wall	Y
Yard/Porch	N

Axonometric Diagram: Shopfront



Section Diagram: Shopfront



Forecourt

The forecourt frontage is allowed in Station Area, Village Center and North Main Street Overlay Districts.

A Forecourt is a public space formed by a recess in the façade of a building. The use of forecourts should be limited to those instances in which they are complementary to the overall pedestrian environment, based on exceptional design and a particular purpose. Forecourts can be appropriate for commercial or civic use, or in some cases for vehicular drop-off at a civic building or hotel, as distinct from courtyards that are semi-public spaces providing frontages of a generally residential character.

A great variety of forecourt designs are possible, per the following (see axonometric and section diagrams):

- A** min. 10 feet clear¹, max 60 feet deep (clear) from the required building setback.
- B** min. 20'; max 60'
- C** The court may also be raised from the sidewalk, not to exceed 3 feet from the adjacent sidewalk grade.
- D** Storefronts shall be at least 10 feet tall.
- E** The corresponding storefront(s) opening(s) along the primary frontage shall be at least 65% of the 1st floor wall area and not have opaque or reflective glazing.
- F** A physical transition shall be provided between the glazing of the storefront and the grade except if the glazing itself terminates directly at the grade. Where a bulkhead is to transition between the opening(s) and the adjacent grade, the bulkhead shall be between 10 inches and 36 inches tall (aluminum storefront or spandrel panel may not substitute for a bulkhead).

The storefront shall provide clear views of merchandise displays.

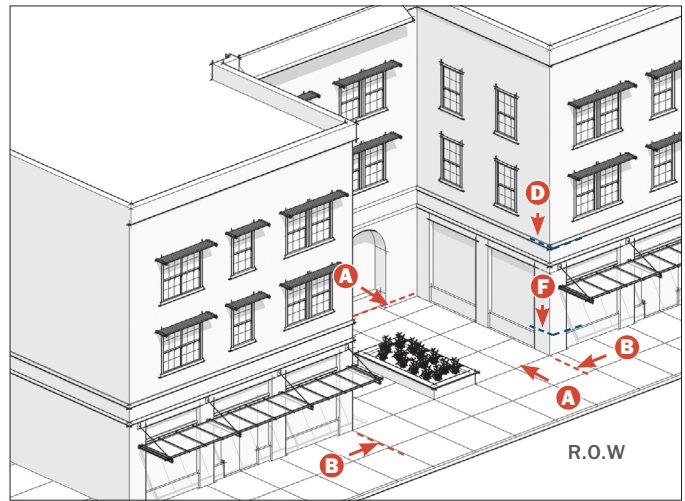
- G** Encroachments within the forecourt, such as balconies, galleries, awnings, signage and light fixtures are allowed up to 1/3 the width and depth of the forecourt.
- H** Minimum clearances for signs, awnings, etc: vertical: 8' from sidewalk; horizontal: width of sidewalk to a maximum of within 2 feet of the adjacent curb.

The following signage types are appropriate on forecourt frontages (see Signage section for more information).

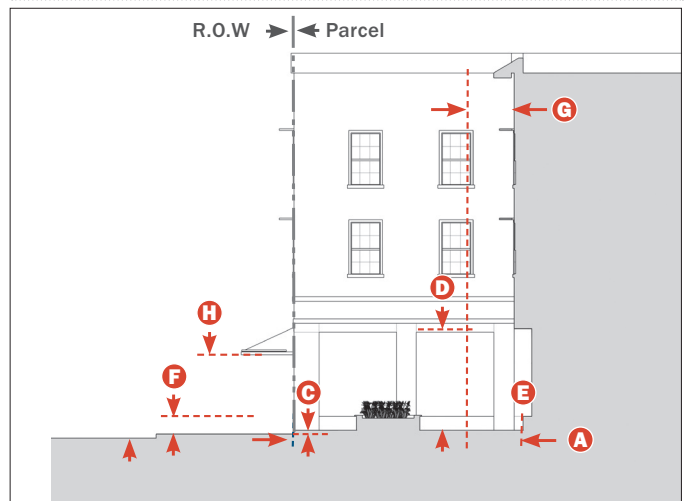
Sign type	Allowed?
Marquee	Y
Awning	Y
Projecting	Y
Wall	Y
Yard/Porch	N

¹ The term “clear” means that the identified area is free of encroachments other than signage, light fixtures, etc.

Axonometric Diagram: Forecourt



Section Diagram: Forecourt



Stoop

The stoop frontage is allowed in Station Area, Village Center and North Main Street Overlay Districts.

Stoops are exterior stairs with landings that provide access to buildings placed close to the frontage line. Building facades are set back just enough to provide space for the Stoop, or the Stoop is recessed within the facade. The exterior stair of a Stoop may be perpendicular or parallel to the sidewalk. A Stoop's landing may be covered or uncovered. Landscaping on either side of the Stoop may be at grade or elevated, and may be demarcated by a garden wall that must not exceed 18 inches in height.

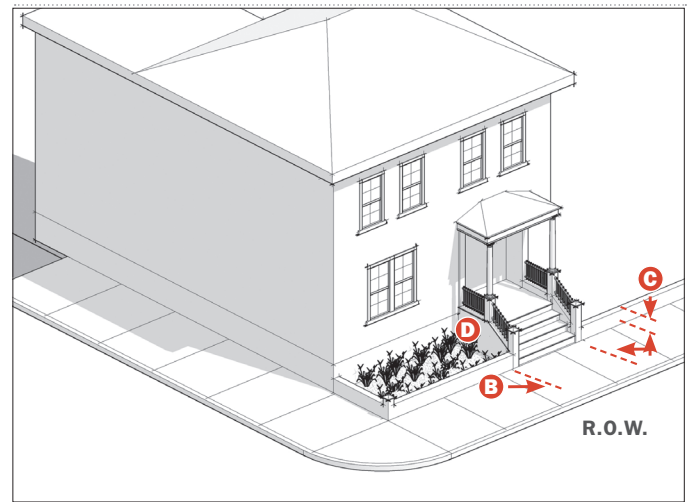
The ground story is elevated from the sidewalk, securing privacy for the windows and front rooms. This type is suitable for ground-floor residential uses with short setbacks. This type may be interspersed with the shopfront frontage type. A porch or shed roof may also cover the stoop. A great variety of stoop designs are possible, per the following (see axonometric and section diagrams):

- A** min. 5 feet clear
- B** min. 4 feet wide clear
- C** Stoops are raised to transition into the building. In no case shall the ground story be elevated more than 3 feet above the adjacent sidewalk.
- D** Stoops must correspond directly to the building entry(s).
- E** Fences or walls defining the stoop or front setback shall not exceed 30" from the highest adjacent finished grade.

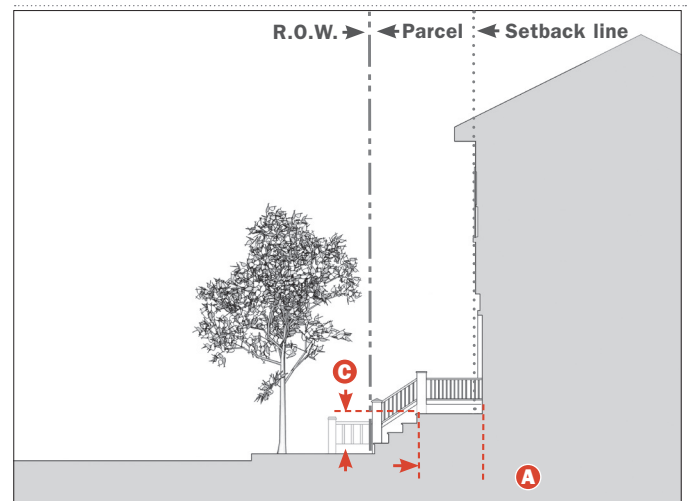
The following signage types are appropriate on stoop frontages (see Signage section for more information).

Sign type	Allowed?
Marquee	N
Awning	Y
Projecting	Y
Wall	N
Yard/Porch	N

Axonometric Diagram: Stoop



Section Diagram: Stoop



Porch

The porch frontage is allowed only in the North Main Street Overlay District. It is not allowed in the Station Area or Village Center Overlay Districts

Porches are covered spaces set back from the right of way with a front yard between the sidewalk and the porch raised, or at grade, that provide a physical transition from the sidewalk to the building. A fence or wall at the property line may be used to define the private space of the yard. The front yard may also be raised from the sidewalk, creating a small retaining wall at the property line with entry steps to the yard.

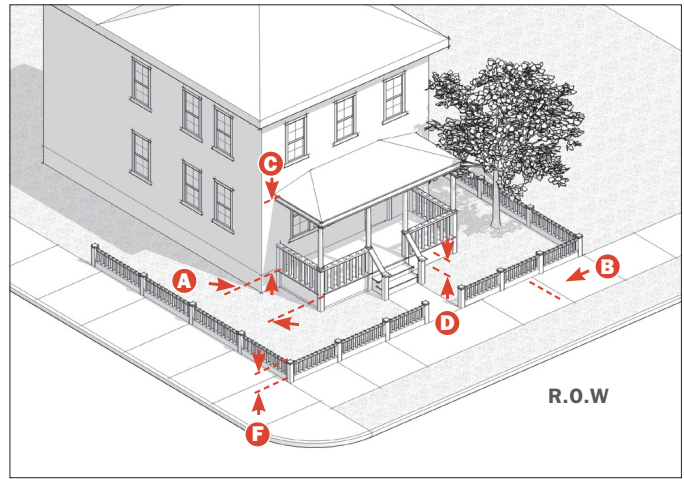
A great variety of porch designs are possible, per the following (see axonometric and section diagrams):

- A** Depth: min. 7 ft clear
- B** Width: min. 12 ft clear for centered entry; min 10 ft for asymmetrical entry
- C** Height: min. 10 ft clear
- D** Porches may be at grade or raised to transition into the building. In no case shall porches be raised more than 3 feet from the adjacent grade.
- E** Fences or walls defining and/or retaining the front yard shall not exceed 3 feet in height from the adjacent sidewalk.

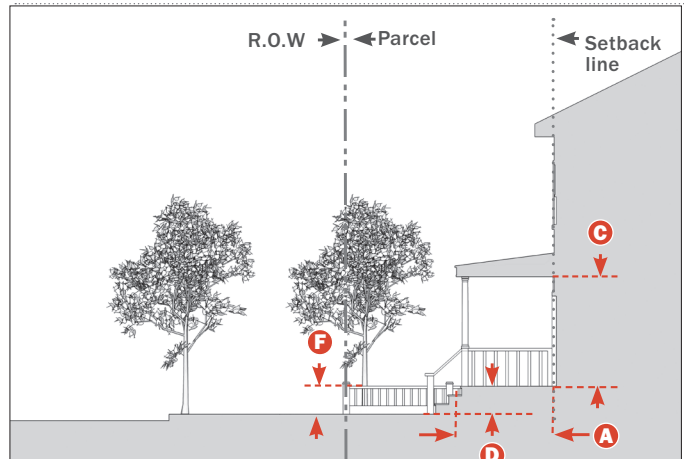
The following signage types are appropriate on shopfront frontages (see Signage section for more information).

Sign type	Allowed?
Marquee	N
Awning	Y
Projecting	N
Wall	Y
Yard/Porch	Y

Axonometric Diagram: Porch



Section Diagram: Porch



Signage

The sign regulations in this section of the code are intended to appropriately identify and allocate the placement, type, size, and number of signs allowed within the Station Area, Village Center and North Main Street Districts, and to require the proper maintenance of signs. The purposes of these requirements are to promote the aesthetic and environmental values of the community; and provide for signs as an effective channel of communication, while ensuring that signs are aesthetically proportioned in relation to adjacent structures and the structures to which they are attached.

This Section does not regulate the message content of a sign, regardless of whether the message content is commercial or non-commercial.

General requirements for all signs

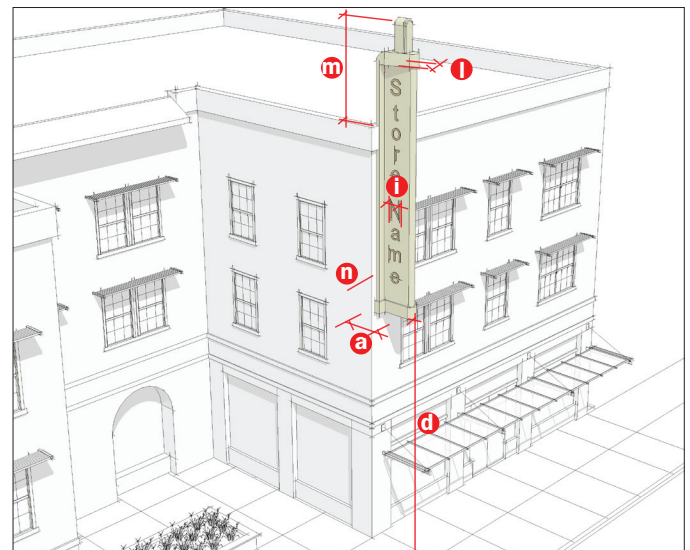
- The measurements of sign area and height are described hereafter, according to signage type. Signs shall be located on the same site as the subject of the sign.
- Each sign shall comply with the setback requirements of the applicable Overlay District, except for approved projecting signs, and approved freestanding signs. Signs cannot be placed so as to interfere with the operation of a door or window or over prominent architectural features of the building.
- Signs should be integral with and subordinate to the architecture of the building on which they are placed.
- Sign elements and lettering should be in proportion with their location on the building.
- The use of individual letters incorporated into the building design is encouraged, rather than a sign with background and framing other than the structure wall. Wherever possible, graphic elements shall be incorporated into signage. Awning signs and projecting signs are also encouraged. Ground-mounted signs are strongly discouraged, except for small wood signs that are mounted on decorative posts in the front yards of houses. Roof signs shall be allowed in limited circumstances along Sunrise Highway and Merrick Road.
- Except for banners, flags and temporary signs, signs shall be constructed of permanent materials and permanently affixed to the ground or building. Materials for permanent signs shall be durable.
- The Village does not regulate the message content of signs; however, sign copy should relate only to the name and/or nature of the business or commercial center.
- Signs shall be designed to minimize light and glare on their surroundings. Front-lighted signs with decorative wall-mounted fixtures are preferred. Sign lighting shall not blink, flash, flutter, or change light intensity, brightness, or color.
- Temporary signs should be allowed only in very limited circumstances, such as around a particular event or holiday, or to announce the opening of a new establishment.
- All sign types and sizes not expressly allowed are prohibited. Non-conforming signs shall be allowed to remain in use until they are refurbished, replaced or the property is renovated.

Marquee signs

Standards

- A** 6' max (to within 2' of curb)
- B** 24" max
- C** 10' max
- D** may align with corner or be placed anywhere on wall
- E** 12' min
- F** max 50% of 'a'

Signs Per Building: 1 max (not allowed with a roof sign)
Allowed on Shopfront and Forecourt frontages.



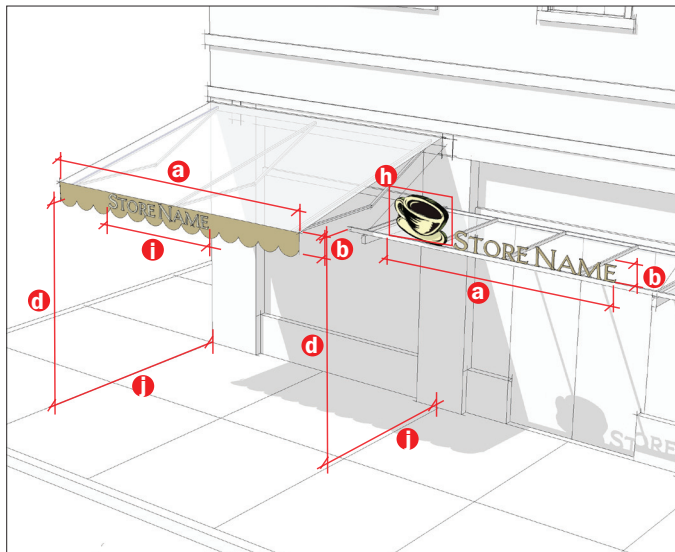
Awning signs

Standards

- A** 20' max (no larger than storefront bay)
- B** 18" max
- B** (1) 10" max
- C** feature area: 6 sq. ft. max
- D** max 50% of 'a'
- E** 8' min
- F** within 2' of curb

Signs Per Building: 1 per awning

Allowed on Shopfront, Forecourt, Stoop and Porch frontages.



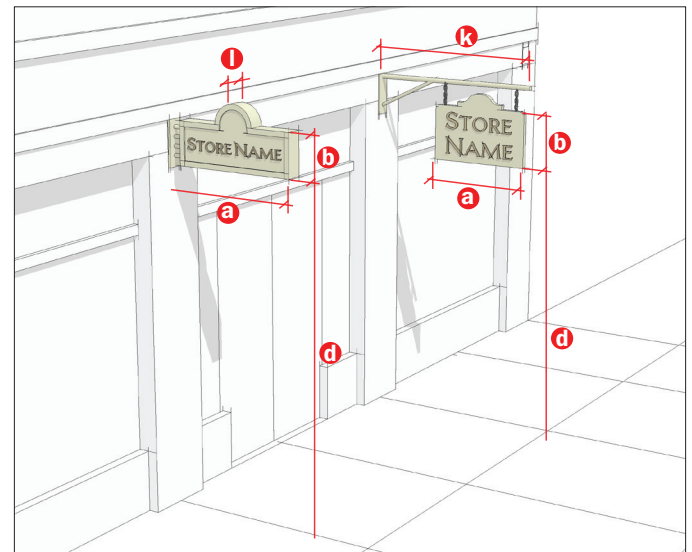
Projecting signs

Standards

- A** 48" max
- B** 30" max
- C** 72" max
- D** 24" max
- E** 8' min

Signs Per Building: 1 per storefront

Allowed on Shopfront, Forecourt and Stoop frontages.



Wall-mounted signs

Standards

- A** width of storefront
- B** 24" max
- C** 18" max if mounted on a background, 24" max if mounted or painted directly on the building wall and front lighted, up to 1 sq. ft. per linear ft. of store frontage

Mountings:

1-story: above 1st floor windows.

multi-story: between windows or above upper-most floor.

Signs per Building: 1 per storefront

Allowed on Shopfront, Forecourt and Porch frontages.



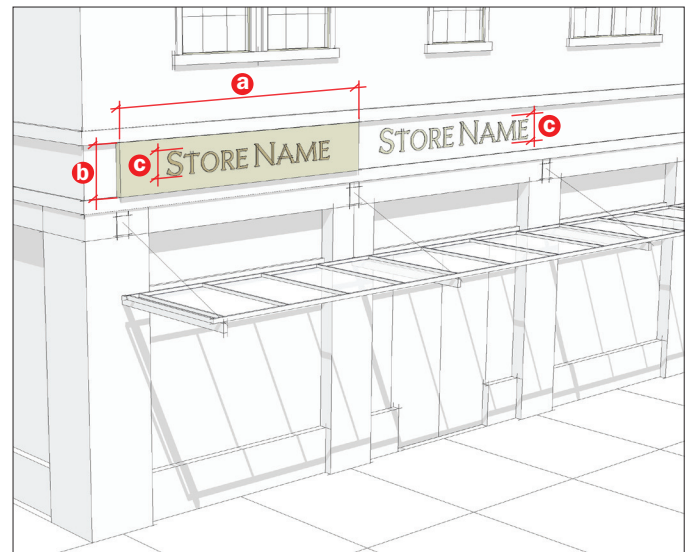
Yard signs (porch or post)

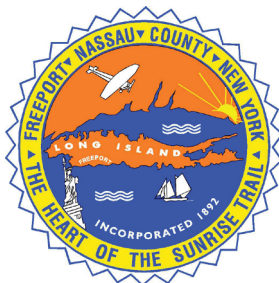
Standards

- A** 36" max
- B** 24" max
- C** (1) 12" min
- C** (2) 8' min
- D** 5' max
- E** 12" max
- F** 12" min

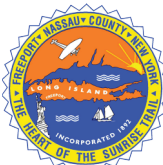
Signs per Building: 1 max

Allowed on Porch frontages.





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ARCHITECTS AND URBANISTS



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