



THE FIVE BOROUGH BIKEWAY

Critical infrastructure
connecting New York City

The Five Borough Bikeway will create a 425-mile city-wide network of protected, continuous, high-capacity, priority bike lanes and serve as the central spine for all bike lanes in NYC.



View the full Bikeway
map at rpa.org

A Leap Beyond the Status Quo

For nearly two decades, NYC has been building out bicycle infrastructure as part of a broader effort to become a more sustainable and equitable city. However, there are a limited number of physically protected lanes — just 480 out of 1,250 miles. Given the increasing demand for our streets, more permanent protected lanes need to be built quickly as part of a unified vision. If NYC is to become a world-class biking city, it's time to take a leap beyond the status quo.

The Five Borough Bikeway will be:

- ▶ **Continuous** — built to prevent hinderances and free of gaps that plague NYC's current network.
- ▶ **Connected** — centrally located and strategically linked to more local bike lane networks.
- ▶ **Conflict-Free** — designed to reduce or eliminate conflicts between bikes, vehicles, and pedestrians.
- ▶ **Constructed** — streets must be altered to create physically separated and protected bike lanes.

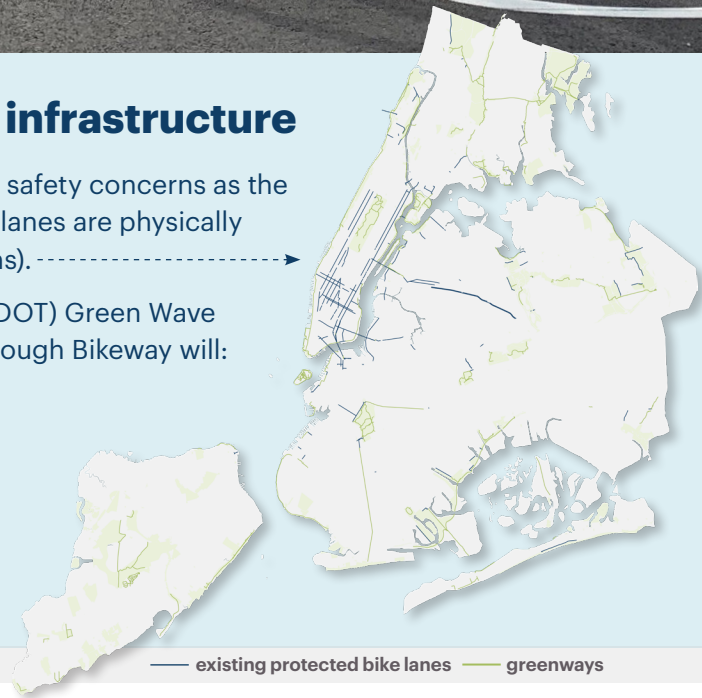


We cannot paint our way to better infrastructure

Nearly two-thirds of those who do not bike regularly cite safety concerns as the main reason, and less than 40 percent of the City's bike lanes are physically separated from road traffic (see map of current conditions). ----->

Building upon the NYC Department of Transportation's (DOT) Green Wave plan and NYC Council's Streets Master Plan, the Five Borough Bikeway will:

- ▶ Increase bike ridership.
- ▶ Reduce crashes and fatalities.
- ▶ Accommodate new modes of micro-mobility.
- ▶ Reduce traffic congestion and air pollution.
- ▶ Make streets safer for everyone.



While fewer than one in ten trips in NYC are made by bike today, there is no reason to think NYC couldn't be a world-class cycling city, with comparable levels of ridership to Copenhagen or Amsterdam.

To create a safe, efficient, and conflict-free bike network, the Bikeway requires the following:

- ▶ A baseline template for design that prioritizes separated bike tracks, reduced-stress intersection design, signal phasing, and clear connections to safe, local streets.
- ▶ An amended public outreach process in order to complete projects faster while better incorporating and prioritizing input from communities of color.
- ▶ A new task force composed of bike advocates, the City Council Speaker, Comptroller, staff from city agencies such as the Department of Design and Construction (DDC) and DOT, and representatives from utility companies.
- ▶ Quick implementation. The first phase could be done quickly with temporary measures like bollards and paint, and should focus on gap-filling along existing but fragmented portions of the network.
- ▶ A broader public works agenda that incorporates bicycle capital projects, sets ambitious annual targets, and hits them. With access to recovery funds, NYC could invest in more capital-intensive greenway lanes.
- ▶ To ensure that bike lanes operate smoothly and safely, a dedicated bike-mounted unit of DOT could be created to address illegally parked cars and delivery vehicles, as well as cyclist and pedestrian infractions.



This report was informed by the Bikeway Advisory Committee, co-chaired by Jon Orcutt of Bike New York and Marco Conner of Transportation Alternatives. RPA thanks the members of the Bikeway Advisory Committee for their ideas and advice, drawn from years of service advocating for and bringing about change on NYC's streets.